

REFERENCE: P/19/915/RES

APPLICANT: Llanmoor Developments Ltd
63-65 Talbot Road, Talbot Green, CF72 8AE

LOCATION: Land west of Maesteg Road, Tondu CF32 9DF

PROPOSAL: Reserved Matters to P/16/366/OUT for 405 residential units, link road and temporary car park

BACKGROUND

This application was to be considered by the Development Control Committee at the meeting on 20 August 2020 but was deferred at the request of the applicant company to allow a review of the drafted planning conditions and more time to resolve outstanding highway related issues. The report reproduced below has been amended to include a summary of the observations received from the community since the publication of the previous report and changes to the planning conditions.

APPLICATION/SITE DESCRIPTION

Sullivan Land and Planning have submitted this Reserved Matters on behalf of Llanmoor Homes, seeking the agreement of all details for a development that includes the erection of 405 dwellings, the construction of a new link road and the formation of a temporary car park on approximately 21 hectares of land located off Maesteg Road, Tondu.

The application follows on from the Outline Planning consent issued on 13 December 2018 which agreed the principle of a development comprising up to 450 dwellings, 1000 square metres of business uses, highway improvement works along with the provision of public open space, green infrastructure, two attenuation ponds and all other associated works (P/16/366/OUT refers). The related S106 obligation secured financial contributions towards primary age education, off-site highway works, sustainable transport initiatives, the delivery of affordable housing and open space, compliance with the design principles of the submitted masterplan and established a management plan for the open space and surface water drainage on site. In accordance with conditions imposed on the Outline Planning consent, details for the following conditions have been agreed:-

- Condition 4: Site Wide Phasing Plan discharged on 14 November 2019 (P/19/633/DOC refers)
- Condition 5: Development Brief discharged on 14 November 2019 (P/19/633/DOC refers)
- Condition 6: Hydraulic Modelling Assessment discharged on 19 September 2019 (P/19/571/DOC refers)
- Condition 9: Strategic Landscaping Scheme discharged on 14 November 2019 (P/19/633/DOC refers)
- Condition 23: Site Investigation Methodology discharged on 13 November 2019 (P/19/595/NMA refers)
- Condition 36: Position and timing of provision of bus stops discharged on 14 November 2019 (P/19/633/DOC refers).

The application site lies within a larger area of 43 hectares identified in the adopted Bridgend Local Development Plan (LDP) 2013 as a regeneration and mixed use development on a former opencast site. Policy PLA3 (10) describes the site as one of a number of brownfield and under-utilised sites within a defined settlement. Some development has already taken place within the area identified in the Policy with residential development to both the north and the south of the application site. Retail uses including a supermarket and smaller units have been built in the southern part of the site along with a Waste Transfer Station.

The wider area is predominantly residential with some commercial uses along Maesteg Road. Parc Slip Nature Reserve adjoins the western boundary of the site. The railway line runs on a north-south line to the east of the main part of the application site and the proposed highway works include a scheme to improve the junction of the A4065 and A4063, close to the railway bridge. The northern portion of the site is located within the Derllwyn Road Conservation Area. There is one Scheduled Monument (Remains of Tondu Ironworks) to the north-east of the application site. There are three Listed Buildings located beyond the eastern edge of the site - two bridge piers on the incline plane and a mile marker on Maesteg Road.

This Reserved Matters submission presents the detailed design proposals for a development of 405 dwellings and a new link road from Ffordd Haearn (Iron Way) exiting at the highway roundabout spur adjacent to the Waste Transfer Station. Primary vehicular access to the site will be a continuation of the existing western arm of the Pentre Felin/Lidl/Proposed Link Road roundabout. Additionally, an emergency access is proposed to the north of the site onto Derllwyn Road.

The Design Principles document approved in discharge of conditions 4 and 5 of the Outline Planning consent established the key placemaking principles for the development that have shaped the details contained as part of this Reserved Matters submission. Overall the site has been divided into four development phases.



Figure 1 – Design Principles Framework

Phase 1, the Main Development Area (MDA), adjoins the existing Llanmoor estate to the east and south, Parc Slip to the west and the SINC and woodland area to the north. The development will comprise 300 dwellings, all two storey in scale and including a mix of 2, 3 and 4 bedroom semi-detached properties, detached properties and terraced units.

The layout of the MDA is formal in character with regular built form providing visual emphasis and continuity along this primary street. Park Square, a central neighbourhood green will form the main 'civic space' for the development connecting to key recreational walking and cycling routes and fronted on all sides by development. It is located off the primary access road for the development and will provide the main circulatory route for buses.



Figure 2 - Main Boulevard and Park Square

The Main Boulevard carriageway and footpaths will be separated by a planted verge with formal, large scale avenue tree planting proposed along the route. Park Square will incorporate both active and quiet areas and accommodate a wide range of activities. Formal tree planting and low formal hedgerows will define the edges of the square with scattered tree planting of mixed native species within its interior. Play will be incorporated formally and informally through Park Square as will the line of a Public Right of Way.

The Main Development Area will also front the Parc Tondu woodland corridor and Parc Slip Nature Reserve and SINC. Here the developer has sought to reduce the density of the development and vary the built form, architectural detailing and street design in a manner that seeks to reinforce the areas distinct and special character. The submitted layout incorporates an ecological buffer zone along the respective edges with the focus of the proposed dwellings being outward looking to benefit from the outward boundaries. A peripheral recreational route will be provided around the Main Development Area connecting into a number of the existing Public Rights of Way. Woodland planting will be within the ecological buffer zones with the area providing a transitional habitat along the woodland edges adjacent to new development. Proposed species selection has sought to complement the existing vegetation in Parc Slip. The layout seeks to preserve and enhance the existing watercourses and ponds and utilise it as part of a natural drainage strategy. Along Parc Slip edge, a mown grass path is proposed through a woodland glade to maximise biodiversity enhancement.

The Pentre Felin Edge is where the new development phase will come closest to existing properties and will be characterised by a linear landscaped corridor through which the proposed recreational route will run. New planting will be undertaken along the existing earth bund that was formed as part of the adjacent development.

The focus of residential buildings is outward to provide surveillance over the landscaped bund. Two parkland areas will be formed on the southern boundary, one of which includes a Local Area of Play.

The Main Development Area will be constructed in two phases (Phase 1a and 1b). The initial phase of development will deliver approximately 130 private dwellings and will also contain the two landscape filtration features shown in the southern parkland area, each of which will discharge either into the watercourse or existing adopted sewers in Pentre Felin. Also included within the southern parkland area are play facilities which will be delivered on completion of the attenuation areas.

The proposed area of open space at the entrance to this phase will also be created as part of a Local Area of Play. Whilst this phase is being developed the construction of the Link Road will be progressed. This is required to be completed before occupation of the 136th dwelling. Phase 1b will commence when Phase 1a has progressed towards completion and will deliver approximately 170 dwellings and 'Park Square' including LEAP/LAP facilities. The Park Square loop will allow the bus services to be extended further into the development

Phase 2 is the Old School Site and lies to the west of Bridgend Road. In recent years it has been used as a car park in connection with the Waste Transfer Station. The proposed development will comprise 30 one and two bedroom flats in a complex of two and three storey buildings designed around a central access and parking area served from a new access created off the stopped up section of highway fronting 47/48 Bridgend Road. The massing, scale and layout has been revised to create 'landmark' buildings at the new road interchange that will be formed through the construction of the new by-pass and re-aligned link road.

A terrace of 3 bedroom dwellings will also be developed as part of this phase with access directly from Maesteg Road. The design of the units follows the architectural rhythms of the adjacent church building in terms of form and scale. This site is more urban in character and can support the higher density of development proposed. Areas of green space will frame this phase of the development either through the retention of an existing wooded area or new tree and hedge planting along the western and southern flanks of the site. Phase 2 will include a high proportion of affordable housing and is likely to be delivered when the link road has been constructed and the stopping up works on Maesteg Road completed.

Phase 3 is partially located within the Derllwyn Road Conservation Area and fronts, in part, Derllwyn Road and the allotment gardens to the rear of Park Terrace. The site also shares its boundary with the Iron Works (Scheduled Ancient Monument) to the east. The development will comprise 72 dwellings, again being a mix of house types including 2 bedroom links and larger 4 bedroom detached units. Access from the Main Development Area will be along a new section of road that will cross the woodland and national cycle route and connect to the estate network.

A pedestrian/cycle route will be constructed on the north western edge of this phase connecting the highway network at Derllwyn Road to the site and National Cycle network. The road has also been designed to accommodate the 'emergency access'. Under the terms of the Outline consent this access should be available before the occupation of the 200th dwelling. This phase of the development, according to the Design Principles document, should have a pleasant and quiet character created by its limited access points and enclosed woodland setting.

The site's topographical character falls from Derllwyn Road Conservation Area down towards the former Iron Works. Significant earth works and vegetation clearance will be necessary for the construction of this phase of development, acknowledged at the time of the Outline consent and controlled through the conditions imposed.

The development will be outward facing fronting Derllwyn Road, the National Cycle Route and the woodland edges. Along the boundary with Derllwyn Road, a semi-formal transitional space is proposed incorporating retained vegetation and new planting. Landscape treatment has been designed to deter anti-social behaviour along development edges through planting on the northern side of the active travel route only so residential properties overlook this edge. These proposals have been enhanced to soften the impact of the built form on the Conservation Area and the setting of the Listed Buildings on Park Terrace. The woodland character on the sloping edge of this phase with the adjoining the Iron Works will also be enriched with tree planting and woodland meadow planting softening the interface between the new and existing development.

Development of Phase 3, the Derllwyn Road area, will commence as the development of Phase 1b progresses. The early commencement of development within this area is partly to deliver the emergency access point at occupation of the 200th unit. This is required within this part of the development unless an alternative is approved by the Council. Development of Phase 3 will comprise approximately 70 units including the balance of any required affordable housing. It will also deliver a contribution towards the upgrade of the adjacent Play Area at Derllwyn Road. Surface Water drainage will discharge to the existing ponds in the woodland area and Foul Drainage will connect to the sewers in the Purple land.

The designer's response to the Derllwyn Road Conservation Area is a variance in the built form with greater architectural detailing and more limited material palette but including the use of recon-stone detailing with pennant grey and red hues and rendered facades with stone lintels and quoins. Low recon-stone walls and hedgerows will define the site's boundary with Derllwyn Road to help create a distinctive sense of place. Roof material along the Derllwyn Road area will be slate with the remainder within the phase slate grey in colour. Chimneys will also be introduced to help vary the roof profile and complement with traditional properties along Derllwyn Road area

The materials and architectural detailing of all the phases will match the local palette of materials used in the adjacent development. The principal material used will be red or buff brickwork with the occasional use of render and stone on feature buildings. Some contrasting materials such as weatherboarding, stone and render have been introduced at a number of locations. As discussed above, the palette for the phase within the Conservation Area will differ.

Car parking for all phases has been designed to accord with the requirements of the Council's Supplementary Planning Guidance and reflects the number of units and number of bedrooms. Unallocated visitor parking will be evenly distributed across the site with spaces located on or near the road frontage.

The proposed development will provide a network of open spaces and green infrastructure that create space for play and community interaction. The main focus areas for open space will be at the entrance to the main development area (Arrival Space POS1), at the centre of the site (Park Square POS2) and along the southern boundary of the site (Southern Parkland POS3). These spaces will be over-looked by dwellings and will incorporate formal play equipment, natural play and will incorporate surface water landscape detention basins that provide amenity and biodiversity benefits.

This Reserved Matters submission also includes details of the by-pass and link road that were requirements of the Outline Planning consent and policies of the Bridgend Local Development Plan. The bypass road will commence its construction at its northern end, roughly opposite the vehicular access currently serving TM Car Sales. The new road will roughly follow a south westerly direction, passing the modified 'T' junction with Ffordd Haearn (Iron Way). The alignment of the new road away from the eastern side of Maesteg Road necessitates the formation of a new junction with a stopped up section of Maesteg Road. Some 25m from the stop line of this junction a secondary access will be formed to serve the stopped up section from the junction of Cross Street to 36 Maesteg Road.

Between the new section of roads and their respective junctions and behind a 1.6m high wall, an area of car parking will be formed creating nine spaces on the western side of the carriageway. It will serve approximately 9 properties (37-45 Maesteg Road). The proposed arrangement also retains vehicular access to 36 Maesteg Road and will include a small turning area.

From the newly formed junction of Maesteg Road with the by-pass road, the 7.3m wide carriageway will align to follow the remaining section of stopped up highway from 45 to 49 Maesteg Road which also serves the Wesley Methodist Church and Church Hall on the western side. Phase 2 of the development known as the Old School Site will also be served from this section of highway. At the termination of the road, on-street parking spaces will be created along with a series of cycleways/footways connecting to a new network to be constructed alongside the re-aligned linkroad which will be constructed from the railway bridge to the Pentre Felin/Lidl roundabout.

The northern spur on the Pentre Felin/Lidl roundabout is the southern end of the new by-pass road. From this point the road, with its 7.9m wide carriageway and 3.5m wide Active Travel route on its eastern side, will follow a north/north easterly route rising through a wooded area that will be cleared for construction and passing immediately adjacent to the Grade II Listed stone bridge abutments. A light controlled crossing will be formed at this location to allow pedestrians and users of the National Cycle Route to cross the by-pass road. A Heritage Impact Assessment which specifically considers the impact of the highway construction on the Listed structures has accompanied the application – the findings will be considered later in this report.

As the Reserved Matters submission seeks the agreement of all details, the application has been accompanied by detailed soft landscaping plans for all phases of the development. The design incorporates retained trees where possible with any removed off-set by new planting. Habitat buffers and ecological corridors will provide a landscape buffer where the development meets the woodland edge. Enhanced boundary planting will also be provided along the retained bund to screen the development from existing properties on Pentre Felin.

A comprehensive submission of external works drawings, house types, cross-sections and the details of all enclosures have accompanied this application.

RELEVANT HISTORY

P/16/366/OUT – Outline Planning consent was granted for a development comprising up to 450 dwellings, 1000 square metres of business uses, highway improvement works along with the provision of public open space, green infrastructure, two attenuation ponds and all other associated works – Conditional consent was issued on 13 December 2018.

PUBLICITY

The application has been advertised in the press and on site as a major development but one that also affects the setting of a Listed Building.

Neighbours have been notified of the receipt of the application and again following the submission of amended plans on 7 August 2020. The period allowed for response to consultations/publicity will expire on 21 August 2020.

CONSULTATION RESPONSES

Transportation Development Control Officer: No objection subject to conditions and informative notes.

Land Drainage Officer: No drainage information has been provided however, Section 3.7 of the Design Compliance Statement states '*Drainage features throughout the site will provide amenity space and biodiversity either by natural forms or by use of specialist landscaping proposals. SUDS features will be located within the Park Square and Southern Park. The existing ponds located within the woodland area in the north of the site will also function as flood out areas. Drainage for the link road will discharge into the existing watercourse and/or adopted sewers.*' The applicant has been in discussion with the land drainage team regarding discharge rates and have been advised that discharge rates shall be limited to Qbar for all storm events to ensure the flood risk to downstream is not increased. Existing drainage networks should not be removed and any watercourse should remain open, with culverting only for access purposes. In addition to the requirements of the Outline Planning consent, the applicant will be required to provide a construction management plan which outlines how the existing developments downstream will be protected from surface water/ground water issues.

Rights of Way Officer: In view of all the information provided in relation to the potential future provision of the public rights of way and cycle route network within the application site, the Rights of Way Section has no objection to the approval of the Planning application.

Building Conservation and Design Officer: Subject to further pre-application discussion with the applicant to inform a future Listed Building Consent application to include proposals for the comprehensive schedule of repairs and interpretation of the Listed structure(s) and the inclusion of the suggested conditions, the proposal can be supported.

Economy and Natural Resources Manager: The application as submitted doesn't include documentation to satisfy the nature conservation conditions associated with P/16/366/OUT. Therefore, I have no observations at this current time.

Natural Resources Wales: We have no comments to make regarding the above Reserved Matters application further to our response dated 13 June 2016 in relation to the original application P/16/366/OUT. We look forward to being consulted on the discharge of conditions in relation to P/16/366/OUT in due course.

Dwr Cymru Welsh Water: We acknowledge this is a Reserved Matters application for 405 dwellings approved under Outline Planning permission (ref: P/16/366/OUT). Whilst we confirm that we have no objections to this, acknowledge condition 7 of the original permission which requires the agreement of a comprehensive and integrated drainage scheme for the site before development commences. Therefore we kindly request to be consulted on any future application submitted to discharge condition 7 of the original Planning permission.

South Wales Police (Designing out Crime Officer): The Design and Access Statement for the proposed development states that the developer intends following the principles of 'Secured by Design'. I have provided a report that gives advice that if followed would allow the development to achieve the Secured by Design Award.

The Bridgend Ramblers: No objections.

REPRESENTATIONS RECEIVED

Cllr James Radcliffe – Local Member: I object to this application and request the matter is referred to Committee due to the considerable public interest in this application.

Cllr Radcliffe has made a detailed submission on changes he would wish to see to the scheme. The substance of Cllr Radcliffe's comments are set out as follows:

- “An increase in affordable housing from 10% to 20%
- Temporarily reserving space on the development for a GP surgery/health centre for a limited period (say 2 years). If the authority and NHS are unable to finance/agree to the creation of the health centre this condition expires. This means that after this period either the developer proceeds with the original plans or gets a health centre to make the estate attractive to buyers (in 2006 I understand that the original plans included a health centre but this fell through).
- Scrapping the link road and replacement with (cheaper) alternative scheme to mitigate traffic – this saves the developer money to make amendment 1 viable, and probably makes the scheme more profitable, as well as more acceptable and consistent with recent policy on climate change and active travel.

I would rather keep the wording of the amendment on what the alternatives were as flexible as possible (like some of the conditions for the Paper Mill, which were dependent on the outcome of a bid for active travel). However, for the purpose of an example of the thinking that we have done, below are proposed alternatives for traffic mitigation that don't require the construction of a new road (and I am not wedded to these proposals, they have been discussed locally as preferable but may need adjustment):

North of the Tondu Bridge:

- Mini Roundabout by retail park made more suitable for heavy goods vehicles (road widening)
- Pedestrian crossing at intersection of Derllwyn road and A4063 – slows traffic down and tackles dangerous blind spot.

South of Tondu Bridge:

- Traffic lights south of the bridge to change to purely a pedestrian crossing. This means no right turn into Aberkenfig (from the north) or Bryn Road (from the South) to be allowed on A4063 at this location.
- A Left turn only for cars exiting Aberkenfig or Bryn road here.
- Left turn only into Aberkenfig or Bryn Road from A4063 at this junction
- Additional Investments in active travel routes to Tondu School, Shops in Aberkenfig, and the train stations (i.e. CCTV, proper lighting, Pedestrian crossing for main road in Aberkenfig)
- If feasible, Active travel routes between Sarn Train Station and Macarthur Glen alongside A4063 – this would then combine with existing walking/cycle routes to ensure active travel routes from the new estate, Aberkenfig, and Sarn - to Macarthur glen (as well as effectively linking Macarthur glen to the rail network).”

Councillor Tim Thomas – Local Member: Queries the affordable housing contribution, traffic control measures and the ability of roundabouts to accommodate HGVs.

In a response to the most recent consultation **Cllr Tim Thomas** provided the following observations:

'I object due to the inadequate road lay out provided on the A4063/Maesteg Road and Bryn Road/Bridgend Road junction - this road regularly has traffic queuing back to the B4281/A4063 roundabout and cannot cope with the normal daily volume of traffic, adding 405 households extra of traffic will make a terrible situation worse. The proposed layout will also not be generous enough to large vehicles who will take up both lanes as they try to negotiate the Lidl roundabout. Removal of free traffic flow at lights is a mistake.'

Huw Irranca-Davies MS - Assembly Member for Ogmore passed on a letter of objection he had received from the Tondu and Aberkenfig Community Association.

A joint letter from the **Huw Irranca-Davies MS** and **Chris Elmore MP** has been received requesting that the following issues are taken into account before the determination of the application:

- i. The capacity of the new and proposed highways layout to better accommodate existing and foreseeable future traffic loads, including the congestion at peak times of traffic flow, and including large articulated vehicles (such as those from the paper mills, a manufacturer which will be expanding production soon);
- ii. The capacity of the new and proposed highways layout to properly accommodate active travel for walkers and cyclists, including possible future proposals for developing active travel and safe-routes along the length of the Llynfi valley;
- iii. The preservation of safe-crossings, and also the rights of way network across the roads and also allowing access to the countryside and footpaths;
- iv. The conservation and enhancement of woodland and green space, and the need for green corridors for wildlife and biodiversity;
- v. Sustainable drainage systems across the whole development area, and the impact of changes to the road system and hard-surfaces and raised barriers such as kerbs to the drainage and surface-water flows.

We ask you to take these and other issues raised with you in other representations fully into account in these significant changes to the local area, and to keep local residents and elected representatives fully informed and engaged in these and any related proposals'.

Ynysawdre Community Council: - BCBC have a duty of care to ensure the quality of life/H & S/health of residents are not disadvantaged by inadequate infrastructure.

My councillors have major concerns about the capacity of HGV vehicles using the roundabout at Pentre Felin. Although there are plans to widen the road near the roundabout, it is such a tight turning point. Maesteg Road is already a very busy and dangerous road with 40+ road accidents at the lights and no controlled crossings apart from lights at the junction to Pentre Felin. We feel that there should be improvements to the existing road structure: a dedicated filter light for vehicles turning right off Maesteg Road into Pentre Felin and the same for vehicles coming from Bridgend, a filter light for vehicles turning left into Pentre Felin. Residents have expressed concern at the cost of provision of a new road behind the Wesley Church when adaptations to Maesteg Road may be a better and less expensive option.

Funds then may be used for a new medical centre and village hall as promised in the original plans. Our area is crying out for a new medical centre and the current GP practice in Aberkenfig is inaccessible and full to capacity for patients.

If the new road goes ahead, we wonder if any thought has been given to keeping Maesteg Road open for HGV's and all other vehicles using the new road.

Residents have expressed concern at the emergency exit near Foxtroy nursing home. It seems to be inadequate to have only one access road to Pentre Felin and one small emergency exit. Is there a possibility of creating a further access and egress road onto Fountain Road?

The occupier(s) of the following addresses have submitted their concerns/objections to the scheme:

12 Clos Gwaith Brics
62 and 63 Clos Pwll Clai
Woodlands View & Ty Risca*, Derllwyn Road
1, 3, 4 and 47 Iron Way/Ffordd Haeearn
10 Llynfi Street
38, 40 and 43 Maesteg Road
2 Nicholls Road
2 Park Terrace
1 Pen-yr-heol, Penyfai

*(*Request to speak)*

The observations received from Tondu and Aberkenfig and Community Association* are reproduced in full below:

“Our Members have expressed their concerns regarding the proposed new road link and the impact the re-alignment of the Maesteg Road could have on traffic movements both along the Maesteg Rd and exiting from Pentre Felin and the new estate when it reaches its completion. The final traffic demands on the new roundabout could result in vast bottlenecks. We understand the traffic movements have been the subject of computer modelling but where does that leave us if the reality proves detrimental to expectations. Do we blame the computer or the information it's been fed? Have, for instance, the factored information taken into account not only the increased vehicle movements from the new development but the expected 30% increase in heavy goods vehicles resulting from the expansion at WEPA paper mill.

There is also serious concerns with regard to a suitable pedestrian crossing. The siting of pedestrian refuge crossings do not seem to be a very safe way of crossing such a busy road at peak time, especially if pushing a pram/buggy or with children. It can't all be about the vehicles the residents safety needs factoring in.

Once the traffic lights are removed at the junction between the end of Maesteg Road and the entrance to Pentre Felin and the road re-alignment is complete the opportunity to dash between stationary vehicles along the road will be lost. So a pedestrian crossing is essential, ideally between the bus stop at the bottom of Derllwyn Rd and the Llynfi Arms pub.

Residents of Derllwyn Road have also expressed concern about the added volume of traffic using Derllwyn Rd as a short cut to get through to Sarn and the possibility of heavy construction traffic using the road as access. The provision of an emergency exit onto Derllwyn Rd is the subject of some cynicism from the residents. If the computer modelling is proved wrong, will the emergency exit have its status changed to open access? How will the Emergency Exit onto Derllwyn Road be used and maintained?

The lack of proposed parking spaces for the Waste Transfer Station - 34 spaces is not enough. Hoping that Employees will car share is not a suitable consideration; many already park on the roads alongside and in the road adjacent to the WTS. Will the Council consider placing yellow lines on this road?

Residents who live directly next to the new development are concerned that although the present plans show an enhanced green embankment covered with shrubs/bushes and trees, they feel this may not happen in reality and the plans will be changed, as those of Pentre Felin were, and all we'll be left with is a few dead and dying trees!

These Residents are also concerned how Llanmoor propose to shield their houses from the added noise and disruption?

And in regard to when construction commences, have we a timescale as we are fast approaching the nesting season and how are Llanmoor proposing to protect the existing wildlife?

There is no doubt that the quality of life for residents living in Tondu will be adversely effected by these development decisions, decisions made by people who don't live in the area nor effected by the development. The council's endorsement of these concerns would show some alliance with the residents."

In response to the most recent consultation the Tondu and Aberkenfig and Community Association have provided the following observations:

"Although we may not like it, we understand housing is needed and we applaud Llanmoor Development's sympathetic design. However, the council have a duty of care to ensure that the quality of life and the health and safety of resident are not disadvantaged by inadequate infrastructure.

We trust and expect the authority to have used specialists to develop a sustainable road system, considering the road sector produces the highest level of greenhouse gas and noise pollution.

Those buying these new houses are to be given a voucher towards cycling equipment, but even the most ardent cyclist would not even attempt negotiating the proposed roundabout at peak times, having to cope with 16.5m articulate vehicles in such a tight turning circle Yet aren't we supposed to be encouraging other alternative forms of transport rather than the car.

Maesteg Road is dangerous as it is, with no controlled crossing points apart from the traffic lights at Pentre Felin. The re-alignment of the Maesteg Road offers a Pelican Crossing close to the rear of the Tondu Methodist Church, flanked by bus stops on either side. However, this is to the detriment of the Pentre Felin, who will be expected to manage with refuge crossings. Would you wish to use one, pushing a pram or stroller, walking the dog, or using a mobility scooter at peak times? Yet this is what this re-alignment demands of residents - that is not a reflection of a caring authority. The lack of safe crossings should not be a budget issue, its basic health and safety and environmentally essential. Surely there is a better alternative.

Has thought been given to improving the existing traffic lights at Pentre Felin, by the creation of a yellow box, with a dedicated left turn only filtered lane, for traffic wishing to turn north onto Maesteg Road. Likewise, the addition of a dedicated right turn filtered lane heading south, for those wishing to enter Pentre Felin. Money saved could be used towards providing better Medical and/or Community facilities for the area. And finally, the provision of an emergency exit near the retirement home along Derllwyn Rd is of concern. It seems poor planning to develop a large site with only one acceptable entrance from the Lidl end along with the lack of proposed parking spaces for the Waste Transfer Station.

There is no doubt that the quality of life for residents living in Tondu and Aberkenfig will be adversely affected by these development decisions; decisions made by people who don't necessarily live in the area or will be affected by the development. The council's endorsement of these concerns would show some alliance with the residents."

The occupier(s) of the following addresses have also submitted further concerns/objections to the scheme:

63 Clos Pwll Clai
Woodlands View & Ty Risca, Derllwyn Road
1 Iron Way/Ffordd Haearn
2 Park Terrace
1 Pen-yr-heol, Penyfai

The following is a summary of all objections received from residents:

Transport concerns/highway capacity/highway/pedestrian safety

- Road infrastructure cannot accommodate the additional housing - existing traffic light system cannot currently cope every morning and evening there are long tailbacks on Maesteg Road; increase traffic flow to a transport system that cannot sustain current levels is unacceptable - accidents have taken place on this section of highway; proposed roundabout design will not accommodate HGV traffic – similar to other roundabouts in county borough that are damaged by vehicles;
- it will be almost impossible to access/cross the network from existing properties on Maesteg Road and the Pentre Felin estate; pedestrian crossing points are inadequate; the link road should be constructed before any of the dwellings have been built – original TA did not factor in the traffic that would be generated by the expansion of WEPA; improvements to A4063 should be considered in the interest of pedestrians – the re-alignment will have no benefits; abandon idea of new link road and improve A4063;
- Concern that development could result in additional traffic along Derllwyn Road which is used as a rat-run to avoid the congested traffic light controlled junction;
- Emergency access is proposed onto Derllwyn Road - How will this be controlled and who has a right of access and what constitutes an emergency - Derllwyn Road could not accommodate any additional traffic –on street parking more of an issue since 'lockdown' – likely to be opened up due to congestion on the highway network; design must respect the conservation area setting;
- Parking arrangements for the residents on Maesteg Road and Cross Street are deficient;

- Loss of the Waste Transfer Station car park on Maesteg Road will result in on-street problems of parking on the Pentre Felin estate – the replacement car park is deficient in size;
- Number of residential units will be built on Footpath 17 Newcastle Higher obstructing the Right of Way;

Amenity issues

- Houses too close to Derllwyn Road – development will have an adverse impact on the Derllwyn Road Conservation Area (including Park Terrace)
- the proposed flats on the old school site are a horrendous example of poor quality, poor location housing that will quickly decline into an isolated community island;

Ecology: impact on trees and woods

- Some of the existing trees are formally protected and should not be removed, destruction of the local ecology, loss of natural habitat, ecology survey is incomplete
- Protected species on site, potential impact on the adjacent Nature Reserve;
- Insufficient screening/landscaping;
- The previously proposed wildlife ponds and lacks any pockets of habitat within the two building cores;

Drainage/Flooding issues

- Problems with drainage on the existing Llanmoor Homes site - additional dwellings will lead to greater problems;

Noise and air pollution associated with additional traffic and construction

- Noise and disturbance to residents will be horrendous during the construction process which could last for many years

Existing Medical Services cannot support development

- Impact on the well-being of residents in term of access to health services - existing GP surgery is already overstretched - facilities in the village cannot serve this level of development - part of the adjacent site was earmarked for a medical centre but that has not been developed;
- The Council has relaxed the requirement to provide a health centre on this development; overcrowding at local primary and secondary schools;

Bare minimum of affordable housing being provided

No consideration for PV solar energy, low carbon heating solution or carbon neutral technologies in the construction – lack of wider environmental considerations

COMMENTS ON REPRESENTATIONS RECEIVED

The following comments are provided in response to the other objections that have been received:

Transport concerns/highway capacity/highway/pedestrian safety

The implications of the development on the surrounding network were considered in detail

at the Outline application stage. Members may recall that external consultants and officers of the Transportation and Development Control Section had examined the Transport Assessment that accompanied the Outline Planning application and concluded that overall, the proposed development performed well against the principles of sustainable transportation as identified in Planning Policy Wales. The site was within walking and cycling distance of a number of local amenities in Tondu and Aberkenfig, as well as further afield in Bridgend and other neighbouring communities.

Public transport linkages from the site via bus and train exist. Opportunities to extend the current 70/71 bus route into the site thus minimising the distance from bus stops was also promoted and in this regard it should be noted that the site layout has been designed to accommodate a bus route around the Parc Square and the position of the bus stops have provisionally been agreed. Furthermore, routes from the site to the neighbouring schools and facilities were also deemed to be compliant although the subsequent consent secured improvements to those routes.

Crossing points for existing and new residents were promoted through the Outline Planning application and will be delivered as part of the development and in a manner that will be safe to users. A light controlled crossing on the proposed by-pass road will benefit both pedestrian and cyclists. To aid pedestrian connectivity and safety, a number of formal and informal crossings are also proposed by the development.

The design and layout of the proposed development, supported by the Walking Strategy and an Interim Travel Plan, will facilitate and encourage journeys on foot. The existing National Cycle Network (NCN) Route 4 is well used for recreational cycle trips and is more than likely to be utilised fully for commuting trips by new residents to the area.

The principle of developing the new link road has been agreed and this submission only relates to the agreement of the detail of the road design and junctions etc. A number of representations have been received questioning the design of the roundabout at the southern end of the new link road and whether its design will cater for the levels and type of traffic movements that use this strategic route. The principal objective of roundabout design is to minimise delay for vehicles whilst maintaining the safe passage of all road users through the junction. This is achieved by a combination of geometric layout features that, ideally, are matched to the flows in the traffic streams, their speed, and to any local topographical or other constraints such as land availability. The submitted design has been examined and deemed to be compliant with the Design Manual for Roads and Bridges CD116 Revision 2.

Representations made by a number of parties have suggested that the construction of the new link road be abandoned and that improvements be made to A4063 and surrounding network. Members are reminded that a component part of the allocating policy was the safeguarding of land to deliver an improvement to the transportation network, acknowledging that the section of the A4063 from the traffic signals, beneath the Tondu railway bridge, was sub-standard. To enable the application site to be fully developed, the Council agreed in the Bridgend Local Development Plan and subsequent outline planning consent that the A4063 (Maesteg Road) north of the existing traffic signals would be re-aligned. This current submission accords with both the policy and the outline consent and there is no justification to require such a significant change to the scheme at this stage.

The Transport Assessment that accompanied the outline planning application had a number of objectives, which included measuring the effect of development related traffic on the local highway network and presenting suitable mitigation measures to maximise the development's accessibility and connectivity. It acknowledged that the proposed development would have an impact on local congestion during peak hours but this impact was not deemed to be significant due to the limited number of additional vehicles as compared to the existing situation. The changes in demand were relatively small, peaking at about 6-7 vehicles close to the site every minute.

The Council accepted that the proposed development was not likely to materially add any existing congestion and that any slight harm in terms of congestion would be mitigated by the proposed sustainable transport initiatives proposed by the development. The provision of pedestrian and cycling facilities and appropriate road crossing points, secured by the Outline consent and proposed on the submitted application, will create an environment where less people automatically choose to use their cars but rather to walk, cycle or use public transport which is supported by the Council and national policy.

The assessment did not consider the expansion of the WEPA Paper Mills as that project was not known at that time.

In granting the Outline Planning consent, the Council accepted the impact of the development. The Council's Highways Officer considers that the impact of the proposed development on local highway network would be mitigated by the proposed sustainable transport initiatives which are likely to encourage non-car use and contribute towards a more sustainable means of travel in line with the requirements of local and national Planning policies. Furthermore, the new link road and proposed junction improvements that will be delivered at various stages of the development would ensure that the quantum of housing could be accommodated without there being detriment to highway safety.

The provision of an emergency access is a requirement of the Outline Planning consent. It will be designed and secured to prevent any unauthorised access as it is accepted that Derllwyn Road could not accommodate traffic generated by any phase on this development. The final design will be agreed by the Council before works commence and consideration will be given to design given the location in the conservation area.

The traffic surveys that were part of the Outline Planning application covered Bridgend Road and the northern access into the village of Aberkenfig. The 'T' junction of Derllwyn Road and the A4063 was not however reviewed as it was not deemed necessary. Whilst it is understood that some vehicles may use the aforesaid junction and travel along Derllwyn Road, New Road, Fountain Road and east along the B4281, such a route is far from a short cut. Furthermore, the construction of the link road, junction changes and the removal of a set of traffic lights should increase capacity along the A4063. This should dissuade road users from taking the 'short cut' via Derllwyn Road. Although it is not incumbent upon the developer to provide car parking for the Waste Transfer Station, the Council secured, through the Outline Planning consent, the provision of parking on land to the north west of the existing facility and that does form part of this application. The Council will also assess the need to restrict on-street parking in the vicinity of the site through Traffic Orders and again monies were secured for that process through the S106 obligation secured under the outline consent.

Parking arrangements for the residents on Maesteg Road/Cross Street are deficient

The area formed between the new by-pass road, the junction with Maesteg Road and the stopped up highway fronting 36-44 Maesteg Road will be utilised to provide parking

facilities for the nearby properties and commercial uses. The latest iteration of the proposed arrangements have been examined by the Council's Transportation Engineers and there remains concern that the design will restrict movements for users of the parking area. It is appreciated that there is little scope for generous turning areas owing to the proximity of the highway works but an improved scheme will be a requirement of a Planning condition. In these particular circumstances, the details of the scheme will be the subject of consultation with the residents on Maesteg Road.

A number of Public Rights of Way cross the development site and this was acknowledged at the time the Outline Planning consent was granted. A footpath diversion plan has been submitted as part of the application following discussions with the Public Rights of Way Section. It is intended that the Rights of Way will either follow their original alignment (Footpath 17) or will be diverted along new sections of footpath/cycleways through areas of open space or along sections of the new estate road. Such diversions will be progressed under Section 257 of the Town and Country Planning Act 1990, are subject to consultation and will be considered by the Rights of Way Section of the Council.

Amenity issues

The relationship of the proposed housing to properties on Derllwyn Road has been carefully examined with regard to the safeguarding of living conditions and the potential losses of privacy, overshadowing and domination of outlook. Overall, the proposed design accords with the Council's guideline. The only 'pinch point' of note is where the new units on Plots 334, 337-339 will adjoin the side boundary of Tawelwch (see *Figure 3 – Extract from Site Layout Plan* below):



Figure 3 – Extract from Site Layout Plan

Revised boundary edge cross section drawings have recently been submitted reflecting the changes in the site layout and confirming the finished site levels. The proposed relationship accords with the Council's Design Guide which should ensure that the amenities of the occupiers are protected. The cross-section is reproduced below for Members consideration

:

Figure 4 - Cross-section through Plot 334 and Tawelwch



A review of the proposed site layout relative to existing properties elsewhere is included in the appraisal section.

The Building Conservation and Design Officer advises that the preservation or enhancement of a Conservation Area can be achieved by a development which either makes a positive contribution to an area's character or appearance or leaves them unharmed. This part of the Conservation Area consists of predominantly 19th Century buildings which possess slate roofs, pennant sandstone/rendered walls, plain doors, boundary walls, double hung sash windows and chimney stacks. The pleasant winding street pattern leads to New Road, a simple country road of rural character and great natural beauty and together with the grounds of the demolished Tondu House, they contribute greatly to the interest of the area.

Initial comments provided to the applicant both in terms of the design, materials, landscaping and layout of this part of the development site have been taken on board and have been incorporated into the revised proposal which, on balance, is considered to have a neutral effect on the special interest of the Conservation Area. The subsequent amendment of landscaping proposals for a green buffer/hedge with associated planting together with the retention of existing vegetation on adjacent land, will soften any impact on the setting of the 52 Listed buildings at Park Terrace.

The applicant nevertheless is asked to consider more native hedgerow trees on the space nearest houses and provide further information in relation to a landscape management plan. It is recommended that a condition be included for samples of the proposed materials to be submitted and agreed in the interests of preserving the character and appearance of the area.

Some concerns have been expressed about the design of the housing on the Old School site suggesting that, due to the location, the area will decline into an isolated community. The site does form part of the Outline Planning permission and is closely related to existing housing on Maesteg Road and nearby shops and amenities on Pentre Felin Retail Park. It is acknowledged that the design and layout of the units on the original submission was poor and through negotiations, changes to this phase of development have been made.

Ecology: impact on trees and woods

Concerns have been raised in relation to the effect of the proposed development on the ecology of the area and this will be considered again in the appraisal section. The decision to grant Outline Planning consent was informed by the Preliminary Ecological Appraisal and Level 2 Survey Reports. Both Natural Resources Wales and the Council's Ecologist accepted the findings of the reports although it was necessary to impose conditions that would relate to the protection and enhancement of ecology interest. The reports did not identify an ecological constraint that would prevent the development of this site.

In accordance with conditions 18 and 20 of the Outline consent, a Habitat Management Plan (HMP) and Landscape Ecology Management (LEMP) will be submitted to the Council before any development commences. Both reports will set out the mitigation strategies for habitats and species on site. The submitted landscape scheme has taken the ecology mitigation requirements into account with the buffer zones around the site including suitable habitat to translocate reptiles and dark corridors for foraging bats. Open spaces have also been designed to have an ecological focus.

Despite the concerns expressed, the two pond features in the Parc Tondu Woodland will

be retained albeit reduced in area. The southern park will incorporate not only a Local Landscaped Area of Play (LAP) but also an attenuation feature with associated wetland/wet meadow planting. This accords with the principles agreed at the Outline stage.

Drainage/Flooding issues

The high water table in the locale is one of the factors that will influence the future design of the drainage system and the submitted strategy suggests that positive site drainage will be installed with flow control measures and attenuation as part of a comprehensive scheme for the site that will be agreed by the various land drainage agencies including the Council before development commences.

In granting Outline Planning consent, the Council took account of the potential hazards that contamination presented to this development, the future occupants and the local environment. The report that accompanied the application considered the geotechnical and geo-environmental aspects of the proposed development and where necessary, it provided recommendations for remediation and reclamation strategies in order to make the site suitable for its intended end-uses. Compliance with the recommendations of the report were secured through planning conditions and those conditions will require agreement before any development commences. The Coal Authority also recommended that intrusive site investigations would be necessary to establish the exact situation regarding coal mining legacy issues on the site. Those investigations are still to be carried out. The submitted layout for the respective phases has identified a number of locations where coal mining entries have been recorded. On a worst case scenario, a number of dwellings may be removed from the development but this requires more detailed investigations and consideration by The Coal Authority. Sufficient controls are secured through the permission to safeguard the environment and the safety of future occupiers

Noise and air pollution associated with additional traffic and construction

Local Planning Authorities work closely with pollution control authorities when determining Planning applications. No adverse representations have been received from the Council's Public Protection Section concerning any issues of noise pollution associated with the development. Noise and dust that may be generated through the construction phases will be controlled through other legislation and conditions requiring the agreement of a Construction Management Plan and hours of operation.

Councils are also required to carry out periodic reviews of the air quality in their areas in relation to seven regulated pollutants and to assess this against the air quality objectives set out in the Regulations. Where a Local Authority believes that there is currently or that there is likely to be in future a breach in an air quality objective, it must declare an 'Air Quality Management Area'. No such area has been identified in the location of the application site

Existing Medical Services cannot support development

This is a matter that was addressed at the Outline application stage. Members will be aware that this Council works in partnership with the Health Trust to provide access to health care facilities. Under policies of the Bridgend Local Development Plan, the Council identified sites for new health and well-being facilities. Three sites are identified in the Plan which includes a site at Glanyrafon, Heol yr Ysgol, Ynysawdre. Despite the original Planning permission for development on the adjacent site including a medical centre, it was not formally allocated for such a provision in the Bridgend Local Development Plan and that site has now been developed for other uses. Whilst the Local Planning Authority can identify sites for health facilities, the delivery of medical or dental practices is outside the Council's control. The Health Board are consultees in the preparation of the Development Plan and are made aware of the strategic areas of growth in the County

Borough and the specific housing allocations. As indicated above, the Council can identify sites for new health facilities but it is for the Health Board to manage the provision.

Bare minimum of affordable housing being provided

Policy SP14 of the LDP acknowledges that the provision of planning obligations can affect the viability of residential development and therefore contributions towards affordable housing, open space, education and off-site highway works can be the subject of negotiation. The Outline Planning application was accompanied by a budget appraisal which provided information on the likely costs of the primary site infrastructures, off site highway improvements and site remediation/enabling works required to prepare the site for the proposed development. The high development costs which are estimated to be in the region of £12 million, significantly affected the viability of the site and the level of obligation that the Council could reasonably secure from this development. The strategic importance of this site in delivering regeneration led development with new housing, improved highway infrastructure and commercial development outweighed the benefits of securing all the infrastructural requirements and levels of obligation that are set by the Council's policies and guidelines. In this regard, the Outline Planning consent and related S106 obligation secured the levels of obligation and this application is not an opportunity to review those levels.

The provision of affordable housing to the levels required by policy would have significantly impacted development viability. To have failed to secure any provision would have contradicted local and national policy. Through the permission and agreement the developer is required to deliver 10% of the total number of dwellings on site as affordable housing units, in accordance with an agreed mix, tenure and timetable for delivery.

An objector has identified that the scheme has not incorporated any PV solar energy, low carbon heating solutions or other carbon neutral technologies. The energy efficiency of new dwellings is now controlled under the Building Regulations and whilst it would have been advantageous to couple high efficiency buildings with added passive energy solutions, there is no planning policy requirement to do so and ultimately such measures were not secured as part of the Outline Planning consent. Any future householder may seek to install such features through permitted development rights.

PLANNING POLICY CONTEXT

Up-to-date Local Development Plans are a fundamental part of a plan-led Planning system and set the context for rational and consistent decision making in line with national policies. Planning applications must be determined in accordance with the adopted Plan unless material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004 refers). The Well-being of Future Generations (Wales) Act 2015 places a duty on public bodies (including Welsh Ministers) that they must carry out sustainable development and it is accepted that a plan-led approach is the most effective way to secure sustainable development through the Planning system.

The adopted Bridgend Local Development Plan (2013) (LDP) identifies the settlements of Aberkenfig, Bryncethin, Brynmenyn, Sarn, Tondu and Ynysawdre, collectively defined as the Valleys Gateway Strategic Regeneration Growth Area, as the focus of regeneration led development. The application site is part of a larger allocation for Regeneration and Mixed Use

Development under Policy PLA3 (10) of the LDP. Appendix A1 of the Local Development Plan details the component parts of the policy and this is set out in the table below:

A1.10 PLA3(10) Land West of Maesteg Road, Tondur			
Mixed Use Components	Total Area 43.0 Hectares		Implementation and Funding Source
Policy Reference	Type	Amount	
COM1(31)	Residential	538 units	Implemented and funded through the private sector (including Section 106)
REG1(21)	Employment	1.00ha	
PLA8(3)	Improvement to the Transportation Network	Access to Land West of Maesteg Road	

Policy PLA3 (10) requires that the implementation of each of the schemes identified should be in accordance with a Master Plan or Development Brief along with appropriate Planning and highway agreements.

Under Policy COM1 of the LDP, the application site is allocated for residential development with the policy estimating that 538 dwellings will be constructed on the site within the Plan period including 20% of the dwellings as affordable housing. To date 186 units have been approved within this allocation and the quantum of development proposed by this application exceeds the estimated figure by 98 units.

Under Policy REG1 (21) of the LDP, 1 hectare of land is allocated and protected for employment development on this site falling within Class B1 uses.

As part of this development, a further improvement to the A4063 is required and, to this end, an area of land has been safeguarded for an improvement to the transportation network. From the Council's perspective, the A4063 highway to the south of the nearby railway bridge is up to the necessary standard to accommodate the traffic movements related to the development proposed under the PLA3 (10) allocation however, as it proceeds north from the traffic signals, beneath the railway bridge, bearing sharply eastwards, the highway is sub-standard with poor visibility and several existing accesses. In order that the site can be developed fully and comprehensively, the A4063 (Maesteg Road) north of the existing traffic signals will need to be re-aligned to allow for an appropriate access to be constructed to serve the development (Policy PLA8 (3) refers).

The following non-site specific policies of the Bridgend Local Development Plan (2013) and Supplementary Planning Guidance are also of relevance:

Bridgend Local Development Plan (2013)

- Strategic Policy SP1 Regeneration Led Development
- Strategic Policy SP2 Design and Sustainable Place Making
- Strategic Policy SP3 Strategic Transport Planning Principles
- Strategic Policy SP4 Conservation and Enhancement of the Natural Environment
- Strategic Policy SP5 Conservation of the Built and Historic Environment
- Strategic Policy SP14 Infrastructure
- Policy PLA1 Settlement Hierarchy and Urban Management
- Policy PLA4 Climate Change and Peak Oil
- Policy PLA9 Development Affecting Public Rights of Way
- Policy PLA7 Transportation Proposals
- Policy PLA8 Development Led Improvements to the Transportation Network
- Policy PLA11 Parking Standards
- Policy ENV4 Local/Regional Nature Conservation Sites
- Policy ENV5 Green Infrastructure

Policy ENV6	Nature Conservation
Policy ENV7	Natural Resources Protection and Public Health
Policy ENV8	Heritage Assets and Regeneration
Policy COM3	Residential Re-Use of a Building or Land
Policy COM4	Residential Density
Policy COM5	Affordable Housing
Policy COM11	Provision of Outdoor Recreation Facilities

Supplementary Planning Guidance:

SPG7	Trees and Development
SPG8	Residential Development
SPG12	Sustainable Energy
SPG13	Affordable Housing
SPG15	Community Facilities and Residential Development
SPG16	Educational Facilities and Residential Development
SPG17	Vols. 1 & 2 Parking Standards
SPG19	Biodiversity and Development

Policy SP2 (Design and Sustainable Place Making) in particular states:

All development should contribute to creating high quality, attractive, sustainable places which enhance the community in which they are located, whilst having full regard to the natural, historic and built environment by:

- (1) Complying with all relevant national policy and guidance where appropriate;
- (2) Having a design of the highest quality possible, whilst respecting and enhancing local distinctiveness and landscape character;
- (3) Being of an appropriate scale, size and prominence;
- (4) Using land efficiently by:
 - (i) being of a density which maximises the development potential of the land whilst respecting that of the surrounding development; and
 - (ii) having a preference for development on previously developed land over greenfield land;
- (5) Providing for an appropriate mix of land uses;
- (6) Having good walking, cycling, public transport and road connections within and outside the site to ensure efficient access;
- (7) Minimising opportunities for crime to be generated or increased;
- (8) Avoiding or minimising noise, air, soil and water pollution;
- (9) Incorporating methods to ensure the site is free from contamination (including invasive species);
- (10) Safeguarding and enhancing biodiversity and green infrastructure;
- (11) Ensuring equality of access by all;
- (12) Ensuring that the viability and amenity of neighbouring uses and their users/occupiers will not be adversely affected;
- (13) Incorporating appropriate arrangements for the disposal of foul sewage, waste and water;
- (14) Make a positive contribution towards tackling the causes of, and adapting to the impacts of Climate Change; and
- (15) Appropriately contributing towards local, physical, social and community infrastructure which is affected by the development.

The supporting text to this Policy advises that Policy SP2 demands a high quality of design incorporating equality of access in all development proposals and seeks to ensure that new built development is sensitive to its surrounding environment.

In the determination of Planning applications regard should also be given to the

requirements of National Planning Policy which are not duplicated within the Local Development Plan.

The following Welsh Government Planning Policy will be relevant to the determination of any future Planning application on this site:

Planning Policy Wales (PPW) 10 (December 2018)

TAN 5: Nature Conservation and Planning

TAN 11: Noise

TAN 12: Design

TAN 18: Transport

TAN 23: Economic Development

The primary objective of PPW is to ensure that the Planning system contributes towards the delivery of sustainable development and improves the social, economic, environmental and cultural well-being of Wales, as required by the Planning (Wales) Act 2015, the Well-being of Future Generations (Wales) Act 2015 and other key legislation.

More specifically to this scheme, paragraph 5.4.4 states *Wherever possible, Planning Authorities should encourage and support developments which generate economic prosperity and regeneration.*

In terms of Active Travel, Active Travel (Wales) Act 2013 makes walking and cycling the preferred option for shorter journeys particularly everyday journeys such as to and from a workplace. The Act requires Local Authorities to produce Integrated Network Maps, identifying the walking and cycling routes required to create fully integrated networks for walking and cycling to access work, education, services and facilities.

APPRAISAL

The application is referred to Committee to consider the objections raised by local residents and the Local Member.

This Reserved Matters application follows on from an Outline Planning consent issued on 13 December 2018 and seeks the agreement of all details for a development that includes the erection of 405 dwellings, the construction of a new link road and the formation of a temporary car park on approximately 21 hectares of land located off Maesteg Road, Tondy. The principle of the site being developed as a mixed use development of housing, commercial with associated improvements to the transport network follows on from the site's allocation under Policy PLA3 (10) of the Bridgend Local Development Plan 2013.

The main considerations in the determination of the application are whether the submitted details accord with the requirements of the Outline Planning permission and are acceptable against the aforementioned Policies and design guidance, having regard to all other material Planning considerations.

On the matter of compliance with the Outline consent, the following table is produced for Members consideration. It details all of the Planning conditions with a brief comment on whether this scheme addresses the condition or indeed whether the condition will need to be the subject of further submissions prior to any development commencing on site:

1.	The development hereby permitted shall be carried out in accordance with the following approved documents: a) Application form dated 3 May 2016
----	--

	<p>b) Revised Site Location Plan - EX01 - Revision C provided by C.W. Architects Ltd received on 23 October 2017</p> <p>c) Proposed Pedestrian and Cycle Links - Drawing W152050_B05 Rev B provided by Vectos received on 3 October 2017</p> <p>d) Design and Access Statement, provided by C.W. Architects Ltd received on 9 May 2016.</p> <p>e) SP484 - Stage One Masterplan - Rev A (1:1250 @A1) provided by C.W. Architects Ltd received on 24 April 2017</p> <p>f) Archaeological and Heritage Assessment prepared by The Environmental Dimension Partnership Ltd (EDP)</p> <p>g) E1457501 R01 – Preliminary Ecological Appraisal Final As Issued 2014-12-12 provided by Soltys Brewster Ecology</p> <p>h) E1457501 R02 – Level 2 Survey Report Final As Issued 2016-03-10 provided by Soltys Brewster Ecology</p> <p>i) Preliminary Utility Strategy Issue 2 by ARUP dated April 2016</p> <p>j) Report 15-9428 Tondu Drainage Strategy 3 provided by ARUP</p> <p>k) Tree Survey, Categorisation and Constraints Report by Steve Ambler and Sons Tree Specialist Ltd dated 20 February 2016</p> <p>Reason: To ensure that the impact on amenity and character of the area is acceptable and to mitigate the impact in respect of site drainage, highway safety, contamination, ground conditions, the protection of heritage assets and the sites biodiversity interest.</p>
	<p>Comment: Agreed Design Principles document and Reserved Matters submission accords with the requirements of this condition. Ecology and site drainage are the subject of control through other conditions on this consent.</p>
<p>2.</p>	<p>No more than 450 dwellings and 1,000 sq.m m of commercial (B1 uses) shall be erected on the application site.</p> <p>Reason: To ensure that the final development is in accordance with the Outline submission to ensure that the impact on amenity and character of the area is acceptable and to mitigate the impact in respect of site drainage, highway safety, contamination, ground conditions, the protection of heritage assets and the site's biodiversity interest.</p>
	<p>Comment: Parameters of the development are in compliance with the Outline Planning consent.</p>
<p>3.</p>	<p>The mitigation measures set out in the documents listed below shall be carried out as prescribed in the documents:</p>

	<p>a) Sections 7.0, 8.0, 9.0 10.0 and 11.0 in the Desk Study & Coal Mining Risk Assessment Report Tondu – Rev A April 2016 provided by Integral Geotechnique</p> <p>b) Section 6.0 Conclusions and Recommendations in the Level 2 Survey Report Final As Issued 2016-03-10 provided by Soltys Brewster Ecology</p> <p>c) Section 6 - Recommendations in the Tree Survey, Categorisation and Constraints Report by Steve Ambler and Sons Tree Specialist Ltd dated 20 February 2016.</p> <p>Reason: To avoid doubt and confusion as to the nature and extent of the approved development</p>
	<p>Comment: Measures to be carried out as part of development</p>
<p>4.</p>	<p>Prior to submission of the first of the Reserved Matters applications for residential development of the site, a comprehensive site-wide phasing plan, which accords with the conditions contained in this Notice shall be submitted to and approved in writing by the Local Planning Authority. The phasing plan shall include, as a minimum, the following elements:</p> <ul style="list-style-type: none"> a) the Reserved Matters phases; b) off site highways improvements; c) site accesses; d) major distributor roads/routes within the site; e) footpaths and cycleways; f) strategic landscaping; g) bus stops; h) strategic foul and surface water features; i) open space and play areas j) environmental mitigation measures; <p>The development shall be carried out in accordance with the agreed phasing plan.</p> <p>Reason: To avoid doubt and confusion as to the nature and extent of the approved development.</p>
	<p>Comment: Design Principles document agreed under P/19/633/DOC on 14 November 2019.</p>
<p>5.</p>	<p>Development proposals within each development phase shall be carried out in accordance with a Development Brief which shall have been submitted to and approved in writing by the Local Planning Authority prior to the submission of Reserved Matters for that phase. The Development Brief shall demonstrate how the development will conform to the supporting documents and mitigation measures referred to in Conditions 1 and 3 above.</p> <p>Reason: To avoid doubt and confusion as to the nature and extent of the approved development.</p>

	Comment: Design Principles document agreed under P/19/633/DOC on 14 November 2019.
6.	<p>Prior to submission of the first of the Reserved Matters applications for residential development on the site, a Hydraulic Modelling Assessment shall be submitted to and agreed in writing by the Local Planning Authority to identify a point of connection on the drinking water system. Thereafter the connection shall be made in accordance with the agreed connection option following the implementation of any necessary improvements to the system, as may be identified by the hydraulic modelling assessment. Such works as agreed shall be carried out prior to the occupation of any property on the development site.</p> <p>Reason: To ensure the development is served by an adequate supply of drinking water, to protect the health and safety of existing residents and to ensure no pollution of or detriment to the environment.</p>
	Comment: Details agreed on 19 September 2019 under P/19/571/DOC.
7.	<p>No development shall commence on any phase of the development until a scheme for the comprehensive and integrated drainage of the site, showing how foul drainage, roof/yard water, highway drainage and land drainage will be dealt with has been submitted to and agreed in writing by the Local Planning Authority. The agreed scheme shall be implemented through the various phases of the development and prior to any building being occupied.</p> <p>Reason: To ensure effective drainage facilities are provided for the proposed development.</p>
	Comment: Details will be submitted prior to development commencing
8.	<p>No development shall commence until an Arboricultural Impact Assessment, Tree Protection Plan and Arboricultural Method Statement (in accordance with BS 5837:2012 - Trees in Relation to Design, Demolition and Construction - Recommendations) for the trees affected by the proposed link road and the proposed development have been submitted to and agreed in writing by the Local Planning Authority. The recommendations of the reports shall inform the layout of the phases of the proposed housing and all development thereafter shall be carried out in accordance with the agreed statements.</p> <p>Reason: To minimise the impact of the road construction on the woodland in the interests of the amenities of the wider area.</p>
	Comment: Details will be submitted prior to development commencing
9.	<p>Prior to the submission of the first of the Reserved Matters applications for residential development on the site, a fully detailed strategic landscape scheme, including the</p>

	<p>Informal Public Open Space, Walkways, Eco Corridors (buffer landscape scheme), Open Spaces, Informal Open Space, Retained Woodland and structure planting to the spine road and programme of implementation for the whole site has been submitted to and approved in writing by the Local Planning Authority. The detailed strategic landscape scheme shall be carried out in accordance with the approved schemes and programme and retained thereafter.</p> <p>Reason: In the interests of the residential amenities of future occupants.</p>
	<p>Comment: Details agreed on 14 November 2019 under P/19/633/DOC.</p>
<p>10.</p>	<p>If within a period of up to five years from the planting of any strategic landscaping, any tree or hedgerow planted is removed, uprooted or destroyed or dies (or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective), another tree of the same species and size as that originally planted shall be planted at the same place, unless the Local Planning Authority gives its written consent to any variation.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	<p>Comment: Compliance as part of development</p>
<p>11.</p>	<p>Prior to the development of any development phase, a detailed landscaping scheme for that phase shall be submitted to and approved in writing by the Local Planning Authority. The plans and particulars to be submitted shall include:</p> <ul style="list-style-type: none"> (a) A plan showing the location of, and allocating a reference number to, each existing tree including crown spread, hedgerow and other soft landscape features to be removed or retained on the site. For the purpose of this condition trees are defined as those which have a stem with a diameter, measured over the bark at a point 1.5m above ground level, exceeding 75mm. (b) Details of the species, diameter (measured in accordance with paragraph (a) above) and the approximate height and an assessment of the general state of health and stability of each retained tree and of each tree which is on land adjacent to the site and to which paragraphs (c) and (d) below apply. (c) Details of any proposed crown reduction or lopping of any retained tree or of any tree on land adjacent to the site. (d) Details of any proposed alterations in existing ground levels and of the position of any proposed excavation within the crown spread of any retained tree on land adjacent to the site or within a distance from any retained tree or any tree on land adjacent to the site equivalent to half the height of that tree. (e) Details of the specification and position of temporary fencing (and of any other measures to be taken) for the protection of any retained tree, hedgerow or other soft landscape feature from damage before or during the course of development. <p>In this condition and in Condition 12 below 'retained tree' means any existing tree, hedgerow or other feature which is to be retained in accordance with the plan referred to in paragraph (a) above.</p>

	Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.
	Comment: Details will be submitted prior to development commencing
12.	<p>In accordance with condition 11 (above) all works comprised in the approved details of landscaping, including public open spaces and landscape buffers, associated with each development phase shall be completed prior to the occupation of the penultimate dwelling of that phase and retained thereafter.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	Comment: Compliance as part of development
13.	<p>If within a period of up to five years from the date of first occupation of the last dwelling on a development phase, any retained tree/hedgerow or new tree/hedgerow planted within that phase is removed, uprooted or destroyed or dies (or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective), another tree of the same species and size as that originally planted shall be planted at the same place, unless the Local Planning Authority gives its written consent to any variation.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	Comment: Compliance as part of development
14.	<p>The details submitted pursuant to discharging landscaping as a Reserved Matter shall include a new buffer of woodland, open habitat and scrub at least 10m depth between the proposed development site and the Parc Slip Local Nature Reserve and Site of Importance for Nature Conservation (SINC). The landscaping scheme shall effectively protect the adjacent sites and should include a Landscape Management Plan that details how the new planting and retained habitats will be managed. No gardens of any adjacent dwellings shall be included within the 10m woodland buffer planting zone.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	Comment: Details submitted as part of this reserved matters application – scheme compliant.

<p>15.</p>	<p>The details submitted pursuant to discharging landscaping as a Reserved Matter for any development phase shall provide for the:</p> <ul style="list-style-type: none"> a) Retention and protection of several priority habitats, including wet woodland, ponds and rush pasture (marshy grassland) - Reference the Preliminary Ecological Appraisal Final As Issued by Soltys Brewster Ecology and the Level 2 Survey Report Final As Issued by Soltys Brewster Ecology; b) Maintenance of habitat connectivity through the retention of the woodland along the eastern boundary and new planting bordering the cycleway and Derllwyn Road. Appropriate management of these areas will be required to offset the loss of the north-eastern area; c) A long-term net gain in local biodiversity through the removal of invasive species and the maintenance of a smaller area of marshy grassland habitat; d) Provision of a large attenuation pond in the south-west of the site, an area of more diverse marshy grassland, where careful development of the area will enable the retention of the existing botanical diversity. <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	<p>Comment: Details submitted as part of this Reserved Matters submission – scheme compliant.</p>
<p>16.</p>	<p>The development hereby permitted shall not be commenced and no demolition or partial demolition of any buildings or other structures or the felling of any trees shall take place until a programme and scheme for the carrying out of a survey to identify the presence or otherwise of bats on the site has been submitted to and approved in writing by the Local Planning Authority. The scheme and programme shall take account of seasonal usage and the timing of development on each development phase. The survey shall be carried out in accordance with the approved scheme and programme and shall contain recommendations for measures to protect any bats found before, during and after development, which shall be submitted to and approved in writing by the Local Planning Authority prior to commencement of development. The protection measures shall be fully implemented in accordance with the approved details.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to promote nature conservation.</p>
	<p>Comment: Details will be submitted prior to development commencing</p>
<p>17.</p>	<p>Prior to the commencement of any works within a particular development phase, further surveys shall be undertaken within that area to determine the presence and/or location of, but not limited to, the following species:</p> <ul style="list-style-type: none"> a) badgers and setts in current use b) dormice

	<ul style="list-style-type: none"> c) otters and holts d) water voles e) crayfish f) reptiles (common lizard, slow worm, grass snake and adder) g) great crested newts h) invertebrates survey of key habitats i) breeding birds j) glow worms <p>The surveys shall be carried out in accordance with a programme which shall have been submitted to and approved in writing by the Local Planning Authority taking into account seasonal usage. The results of the surveys along with measures before, during and after development for the protection of any species found, shall be submitted to and approved in writing by the Local Planning Authority. The approved protection measures shall be fully implemented in accordance with the approved details and maintained thereafter.</p> <p>Reason: For the avoidance of doubt as to the extent of the permission granted and to promote nature conservation.</p>
	<p>Comment: Details will be submitted prior to development commencing</p>
<p>18.</p>	<p>The plans and particulars submitted in accordance with the Reserved Matters shall include a Habitat Management Plan (HMP) addressing avoidance, mitigation, compensation, enhancement and restoration of the site and shall include the following:</p> <ul style="list-style-type: none"> a) Purpose and conservation objectives for the proposed works b) Review of site potential and constraints c) Detailed design(s) and/or working method(s) to achieve stated objectives. d) Extent and location/area of proposed works on appropriate scale maps and plans. e) Type and source of materials to be used where appropriate, e.g. native species of local provenance. f) Timetable for implementation demonstrating that works are aligned with the proposed phasing of development. g) Persons responsible for implementing works. h) Details of initial aftercare and long-term maintenance. i) Details for monitoring and remedial measures. j) Details for disposal of any wastes arising from works. <p>No development shall commence until the HMP has been submitted to and agreed in writing by the Local Planning Authority. The development shall proceed in accordance with the agreed details and all features shall be retained in that manner thereafter.</p> <p>Reason: To maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	<p>Comment: The requirements of the condition have informed the submitted layout and landscaping scheme – the condition requires the scheme to be agreed prior to development commencing.</p>
<p>19.</p>	<p>The plans and particulars submitted in accordance with the Reserved Matters shall</p>

	<p>include a Construction Environment Management Plan (CEMP: Biodiversity) and shall include the following:</p> <ul style="list-style-type: none"> a) Risk assessment of potentially damaging construction activities. b) Identification "biodiversity protection zones". c) Practical measures (both physical and sensitive working practices) to avoid or reduce impacts during construction (may be provided as a set of method statements). d) The location and timing of sensitive works to avoid harm to biodiversity features. e) The times during construction when specialist ecologists need to be present on site to oversee works. f) Responsible persons and lines of communication g) The role and responsibilities on site of an ecological clerk of works or similarly competent person. h) Use of protective fences, exclusion barriers and warning signs. <p>No development shall take place (including demolition, ground works and vegetation clearance) until the CEMP - Biodiversity has been submitted to and agreed in writing by the Local Planning Authority. The CEMP - Biodiversity shall be adhered to and implemented throughout the construction period strictly in accordance with the agreed details.</p> <p>Reason: To maintain and improve the appearance of the area in the interests of visual and residential amenity and to promote nature conservation.</p>
	<p>Comment: The requirements of the condition have informed the submitted layout and landscaping scheme – the condition requires the scheme to be agreed prior to development commencing.</p>
<p>20.</p>	<p>The plans and particulars submitted in accordance with the Reserved Matters shall include a Landscape and Ecological Management Plan (LEMP) addressing the following:</p> <ul style="list-style-type: none"> a) Description and evaluation of features to be managed. b) Ecological trends and constraints on site that might influence management. c) Aims and objectives of management. d) Appropriate management options for achieving aims and objectives. e) Prescriptions for management actions. f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period). g) Details of the body or organisation responsible for implementation of the plan. h) On-going monitoring and remedial measures. <p>No development shall take place (including demolition, ground works, vegetation clearance) until the LEMP has been submitted to and agreed in writing by the Local Planning Authority. The LEMP shall also set out where the results from monitoring show that conservation aims and objectives of the LEMP are not being met, how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally agreed scheme.</p> <p>The site shall be developed in accordance with the agreed details.</p> <p>Reason: To maintain and improve the appearance of the area in the interests of</p>

	visual and residential amenity and to promote nature conservation.
	Comment: The requirements of the condition have informed the submitted layout and landscaping scheme – the condition requires the scheme to be agreed prior to development commencing.
21.	<p>No development shall commence on any phase of the development until an invasive non-native species protocol has been submitted to and approved in writing by the Local Planning Authority detailing the containment, control and removal of Japanese Knotweed on site. The measures shall be carried out strictly in accordance with the approved scheme and throughout the development of the respective phases.</p> <p>Reason: To maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	Comment: Details agreed on 13 November 2019 under P/19/460/DOC
22.	<p>Prior to the development on any phase, a detailed lighting strategy for that phase shall be submitted to and approved in writing by the Local Planning Authority. The lighting plans shall be implemented as approved.</p> <p>Reason: To maintain and improve the appearance of the area in the interests of visual amenity and to promote nature conservation.</p>
	Comment: The requirements of the condition have informed the submitted layout and landscaping scheme – the condition requires the scheme to be agreed prior to development commencing.
23.	<p>Prior to the undertaking of any site investigation works in respect of Coal Mining Risk, the methodology for such works shall first be submitted to and agreed in writing by the Local Planning Authority in respect of the following:</p> <p>(i) A scheme of intrusive investigations for mine entries or the submission of evidence that they have been removed by the surface mining operations;</p> <p>(ii) A scheme of intrusive site investigations for the shallow coal workings;</p> <p>(iii) A layout plan which identifies appropriate zones of influence for the mine entries on site and definition of suitable no build zones, following a detailed review of available information and based on a worst case scenario;</p> <p>No development shall commence within any individual phase until the results of the site investigation including any scheme of treatment/remedial works required for both the mine entries and shallow coal mine workings to ensure the safety and stability of the proposed development has been submitted to and agreed in writing by the Local Planning Authority. This submission shall include a programme for implementation of the aforesaid works.</p>

	<p>The agreed treatment/remedial works shall be implemented on site prior to the commencement of development of the phase to which they relate)</p> <p>(P/19/460/DOC – Details agreed 13 November 2019 by Bridgend County Borough Council)</p>
	<p>Comment: The methodology for undertaking the site investigation works has been agreed on 13 November 2019 under P/19/460/DOC. The results of the site investigation works and any remedial works required will need to be agreed before any development commences.</p>
24.	<p>No development shall commence on any phase of the development until a detailed remediation scheme and verification plan to bring the site to a condition suitable for the intended use by removing any unacceptable risks to human health, controlled waters, buildings, other property and the natural and historical environment has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include all works to be undertaken, proposed remediation objectives and remediation criteria, a timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation. All work and submissions carried out for the purposes of this condition must be conducted in accordance with DEFRA and the Environment Agency's 'Model procedures for the Management of Land Contamination, CLR 11' (September 2004) and the WLGA/WAG/EA guidance document ' Land Contamination: A guide for Developers' (July 2006), unless the Local Planning Authority agrees to any variation.</p> <p>Reason: To ensure that any unacceptable risks from land contamination to the future users of the land, neighbouring land, controlled waters, property and ecological systems are minimised and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.</p>
	<p>Comment: Details will be submitted prior to development commencing</p>
25.	<p>The remediation scheme approved under Condition 24 must be fully undertaken in accordance with its terms prior to the occupation of any part of the development. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works. Within 6 months of the completion of the measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be submitted to and approved in writing by the Local Planning Authority. All work and submissions carried out for the purposes of this condition must be conducted in accordance with DEFRA and the Environment Agency's 'Model procedures for the Management of Land Contamination, CLR 11' (September 2004) and the WLGA/WAG/EA guidance document ' Land Contamination: A guide for Developers' (July 2006), unless the Local Planning Authority agrees to any variation.</p> <p>Reason: To ensure that any unacceptable risks from land contamination to the future users of the land, neighbouring land, controlled waters, property and ecological systems are minimised and to ensure that the development can be carried out safely</p>

	without unacceptable risks to workers, neighbours and other offsite receptors.
	Comment: Remediation works undertaken before occupation
26.	<p>In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing within 2 days to the Local Planning Authority, all associated works must stop and no further development shall take place until a scheme to deal with the contamination found has been approved. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme and verification plan must be prepared and submitted to and approved in writing by the Local Planning Authority. Following completion of measures identified in the approved remediation scheme, a verification report must be submitted to and approved in writing by the Local Planning Authority. The timescale for the above actions shall be agreed with the Local Planning Authority within 2 weeks of the discovery of any unsuspected contamination.</p> <p>Reason: To ensure that any unacceptable risks from land contamination to the future users of the land, neighbouring land, controlled waters, property and ecological systems are minimised and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.</p>
	Comment: Remediation works undertaken before occupation
27.	<p>Piling or any other foundation design using penetrative methods shall not be permitted unless otherwise agreed in writing by the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater.</p> <p>Reason: To prevent pollution of controlled waters.</p>
	Comment: Restriction on development – developer to comply with requirements
28.	<p>No development shall commence on any phase of the development until a Construction Environmental Management Plan (CEMP) to minimise dust emissions arising from construction activities on the site has been submitted to and agreed in writing by the Local Planning Authority. The scheme shall include details of dust suppression measures and the methods to monitor emissions of dust arising from the development and shall include the control measures of the air quality. The construction phase shall be implemented in accordance with the agreed scheme with the approved dust suppression measures being maintained in a fully functional condition for the duration of the construction phases.</p> <p>Reason: In the interests of safeguarding the amenities of existing residents.</p>
	Comment: Details will be submitted prior to development commencing

29.	<p>No development shall commence on any phase of the development until a Construction Method Statement (CMS) has been submitted to and agreed in writing by the Local Planning Authority. Thereafter, the construction of the development shall only be carried out in accordance with the approved Construction Method Statement.</p> <p>The Construction Method statement shall address the following matters:</p> <ol style="list-style-type: none"> 1. Details of the phasing of construction works, including timescales; 2. Details of equipment to be employed, operations to be carried out and hours of operation; 3. Mitigation measures to be applied in accordance with the guidance in BS228 (2014) 4. A scheme for implementing effective liaison with the local residents where they are likely to be affected by the noise/vibration at any particular phase of the work and details of how complaints will be dealt with. <p>Reason: In the interests of safeguarding the amenity of residents.</p>
	<p>Comment: Details will be submitted prior to development commencing</p>
30.	<p>No development shall commence until a scheme for the provision of highway mitigation works has been submitted to and approved in writing by the Local Planning Authority. The scheme shall provide improvements to:</p> <ol style="list-style-type: none"> 1. the A4063 (Maesteg Road) / A4065 (Bryn Road) / Bridgend Road, Aberkenfig signalised junction, 2. realignment of the Western arm of the junction to a revised roundabout junction serving the development 3. realignment of A4063 (Maesteg Road Tondu) to create a new link route to the site access roundabout and be in accordance with Vectos Drawing W152050/B/05 Rev C. <p>The scheme shall include:-</p> <ol style="list-style-type: none"> i. Phasing scheme including temporary traffic management proposals ii. revision of all traffic signing, road markings iii. provision of a 3.5m shared footway / cycleway on the Western side of A4065 (Bryn Road) from the signalised junction North to Station Approach, Tondu iv. realigned carriageway markings along the Eastbound arm of the signalised junction and (A4063) to provide an extended merge length to no less than 200m v. provision of a 3.5m shared footway / cycleway on the Northern side of the new Western arm from the signalised junction West to the site access roundabout and spur to the stub end of Maesteg Road, Tondu vi. provision of a 3.5m shared footway / cycleway on the Southern side of the new Western arm of the signalised junction from the existing cycle route connection to 12 Cwrt yr Hen Ysgol West to the site access roundabout vii. provision of an uncontrolled cycleway footway crossing with associated pedestrian refuge between the improved A4063 (Maesteg Road) / A4065 (Bryn Road) / Bridgend Road, Aberkenfig signalised junction and the realigned site access roundabout. viii. provision of a 3.5m shared footway / cycleway on the Western side of the new link road from Iron Way to the site access roundabout ix. signalised Toucan cycle / pedestrian crossing in the vicinity of the existing

	<p>NCN4 with onward 3.5m shared footway / cycleway link to the redundant portion of Maesteg Road, Tondy</p> <ul style="list-style-type: none"> x. Removal of existing bus stops on the redundant portion of Maesteg Road and replacement with new bus stop facilities on the new realigned section of Maesteg Road (link road). xi. Vehicular turning facilities on the southern end of the redundant portion of Maesteg Road xii. New vehicular link from the new realigned section of Maesteg Road to the existing to the redundant portion of Maesteg Road xiii. Replacement off street parking for the loss of the existing parking layby between TM Cars and 37 Maesteg Road xiv. Scheme of waiting restrictions xv. Scheme of 20mph speed restrictions xvi. Scheme of cycle / pedestrian direction signage xvii. Supporting Stage 2 Road Safety Audit <p>The scheme of highway mitigation works shall be constructed in permanent materials in accordance in with the approved details prior to the beneficial occupation of the 136th dwelling.</p> <p>Reason: In the interests of highway network capacity, road safety and promoting sustainable travel patterns.</p>
	<p>Comment: Details included as part of this Reserved Matters submission. Engineering details to be agreed prior to development commencing</p>
<p>31.</p>	<p>Notwithstanding the provisions of the Town and Country Planning General Permitted Development) Order 1995, there shall be no vehicular access to the site other than the two approved access points being the continuation of the existing western arm of the Pentre Felin/Lidl/Proposed Link Road roundabout along the unnamed road and the "emergency vehicles" only access off Derllwyn Road.</p> <p>Reason: In the interests of highway safety.</p>
	<p>Comment: Submitted scheme is compliant.</p>
<p>32.</p>	<p>No development shall commence until a scheme for the provision of a scheme of highway mitigation works has been submitted to and approved in writing by the Local Planning Authority. The scheme shall provide improvements to the geometry of the A4063 (Bridgend Road/Sarn Link)/ B4281 roundabout junction. Such scheme shall include for:</p> <ul style="list-style-type: none"> i. Phasing scheme including temporary traffic management proposals ii. revision of all traffic signing, road markings iii. Supporting Stage 2 Road Safety Audit <p>The scheme of highway mitigation works shall be constructed in permanent materials in accordance in with the approved details prior to the beneficial occupation of the 136th dwelling.</p> <p>Reason: In the interests of Highway network capacity and Road Safety.</p>

	Comment: Details included as part of this Reserved Matters submission. Engineering details to be agreed prior to development commencing
33.	<p>No development shall commence until a scheme for the provision of an emergency vehicular access linking the northern area of the site to the highway network at Derllwyn Road has been submitted to and approved in writing by the Local Planning Authority. The emergency access shall incorporate physical features preventing regular vehicular traffic traversing the route and shall be constructed in permanent materials in accordance with the approved layout prior to the 200th dwelling on the greater development parcel being brought into beneficial use and shall be retained for the free passage of pedestrians and cyclists in perpetuity.</p> <p>Reason: In the interests of highway safety.</p>
	Comment: Compliance as part of site development – details submitted on Reserved Matters submission
34.	<p>No building shall be occupied until that part of the road system which provides access to it has been constructed to at least base course level in accordance with the approved plans.</p> <p>Reason: In the interests of highway safety.</p>
	Comment: Compliance as part of site development
35.	<p>No building shall be occupied until parking has been provided in accordance with details which shall have been submitted to and approved in writing by the Local Planning Authority. Garages meant for parking shall have minimum internal dimensions of 6m x 3m. Notwithstanding the Town and Country Planning (General Permitted Development) Order 1995 (or any order revoking or re-enacting that Order, with or without modification), all such garages and parking spaces shall thereafter be retained solely for the parking of vehicles in connection with the building they serve.</p> <p>Reason: In the interests of highway safety.</p>
	Comment: Garages and car parking space in compliance with this condition.
36.	<p>Details of the position and timing of provision of bus stops throughout the site shall be submitted to and approved in writing by the Local Planning Authority prior to the submission of any Reserved Matters application for the site. Bus stops shall be provided in accordance with the approved details.</p> <p>Reason: In the interests of highway safety and to promote sustainable transport.</p>

	Comment: Details agreed on 13 November 2019 under P/19/633/DOC
37.	<p>No development shall commence on any phase of the development until the applicant or their agents or successors in title has secured agreement for a written scheme of historic environment mitigation which has been submitted by the application and approved by the Local Planning Authority. The approved site investigation works shall be implemented prior to the commencement of development on Phases 4 and 5 or any infrastructure scheme or strategic landscaping area. Thereafter, the programme of work will be fully carried out in accordance with the requirements and standards of the written scheme and the developer shall afford access at all reasonable times during construction to a nominated archaeologist for the purpose of observing the excavations and recording items of interest and finds.</p> <p>Reason: To safeguard the heritage assets that may be buried beneath ground in accordance with national and local planning policy.</p>
	Comment: Details will be submitted prior to development commencing
38.	<p>No development shall commence until a scheme and phasing plan for the provision of a pedestrian/cycle route linking the site to the highway network at Derllwyn Road, adjacent residential development and the National Cycle Network has been submitted to and approved in writing by the Local Planning Authority. The pedestrian/cycle link shall be at a minimum width of 3m, within a corridor of 4m minimum width and provide physical features preventing vehicular traffic traversing along the pedestrian/cycle route and shall be constructed in permanent materials in accordance with the phasing plan and shall be retained for the free passage of pedestrians and cyclists in perpetuity.</p> <p>Reason: In the interests of promoting sustainable travel patterns.</p>
	Comment: Details will be submitted prior to development commencing
39.	<p>The main site spine road of the greater development parcel (Phases 1-5 on the Phasing Plan in the Design and Access Statement, excluding Phase 2/3) shall be laid out to provide a circular route suitable for future public transport to permeate into the site and serve the development. The spine route and circular bus route shall be not less than 6.5m with appropriate lane widening on bends and bus stops.</p> <p>Reason: In the interests of promoting sustainable travel patterns.</p>
	Comment: Compliance as part of site development
40.	<p>The main site spine roads of the greater development parcel (Phases 1-5 on the Phasing Plan in the Design and Access Statement, excluding Phase 2/3) shall be laid out to provide carriageways of no less than 5.5m, with a single cycleway footway of no less than 3.5m and a single footway of no less than 2.0m.</p>

	Reason: In the interests of promoting sustainable travel patterns and highway safety.
	Comment: Compliance as part of site development

Members will note that the Reserved Matters submission complies with those conditions that need to be agreed at this stage.

Policy SP2 of the Bridgend Local Development Plan and the suite of Supplementary Planning Guidance provide a robust framework for assessing the details of this application. All development is required to contribute to creating high quality attractive sustainable places which enhance the community in which they are located. The Policy establishes fifteen criteria against which development proposals will be assessed. As the principle of the development has been established it is considered that criteria 2), 3), 4), 5), 6), 7), 8), 9), 10), 11), 12) and 13) are relevant to this specific proposal and are addressed below:

Having a design of the highest quality possible, whilst respecting and enhancing local distinctiveness and landscape character

In accordance with the requirements of the Outline Planning consent, a Design Principles document has been agreed which translates the key elements of the Design and Access Statement and Masterplan from the initial consent and establishes key placemaking principles that have informed this Reserved Matters submission. In that regard, the submitted details demonstrate a cohesive design which should create a place with character that will enhance the natural environment through the provision of green infrastructure, delivering benefits for biodiversity through creation, enhancement and sensitive management of habitats. The proposed housing will be sustainably located close to recreational and retail facilities with an integrated and accessible transport system.

Where the higher test of design is required in the Derllwyn Road Conservation Area, the layout, architectural detailing and material palette will ensure that the development preserves and enhances the Conservation Area. Sensitive boundary treatment and an enhanced planting scheme along the north western edge with the rear of Park Terrace will soften the impact of the development on the setting of the Listed Buildings of Park Terrace.

Being of an appropriate scale, size and prominence and Using land efficiently by: (i) being of a density which maximises the development potential of the land whilst respecting that of the surrounding development; and (ii) having a preference for development on previously developed land over greenfield land;

The Outline Planning consent and Design Principle document established that the proposed building heights and scale of the development would be informed by the character and scale of the existing housing together with the landscape context and topography.

A mixture of detached, semi-detached, short terraces and flats are proposed. Generally, all dwellings will be two storey in height apart from the former school site where a complex of two and three storey buildings will occupy the corner of this gateway site overlooking the roundabout junction that will connect the new by-pass and link roads. The relative levels of the site, the retained landscaping and the distances from existing properties enables the increase in storey height. On all other phases of the development, the scale and size of the housing respects the context including the Derllwyn Road Conservation Area. Prior to works commencing on the respective phases details of the finished floor levels will be agreed as an additional safeguard.

Over all the phases of development, medium density levels will be achieved which is comparable to the surrounding development. It should be noted that the total number of units proposed as part of this application is below the 450 units approved at the Outline stage. As indicated above, an increase in density on the Old School site is supported and is off-set by a reduction in building densities along the southern and eastern boundaries which transition to area of open space.

Providing for an appropriate mix of land uses

The parameters of the development and the mix of uses were established by the Outline Planning consent and in this regard the development is compliant. The commercial area, (Phase 4) will be submitted as a separate Reserved Matters submission. A temporary car park will however be formed on this land to accommodate the over-spill parking associated with the Waste Transfer Station. This will be accessed directly from the main access road.

Having good walking, cycling, public transport and road connections within and outside the site to ensure efficient access

All development proposals should be designed in a manner that secures the safety of all highway users whilst also looking to improved opportunities for sustainable travel. The proposed movement and access arrangements for the development site are illustrated on the extract plan below:

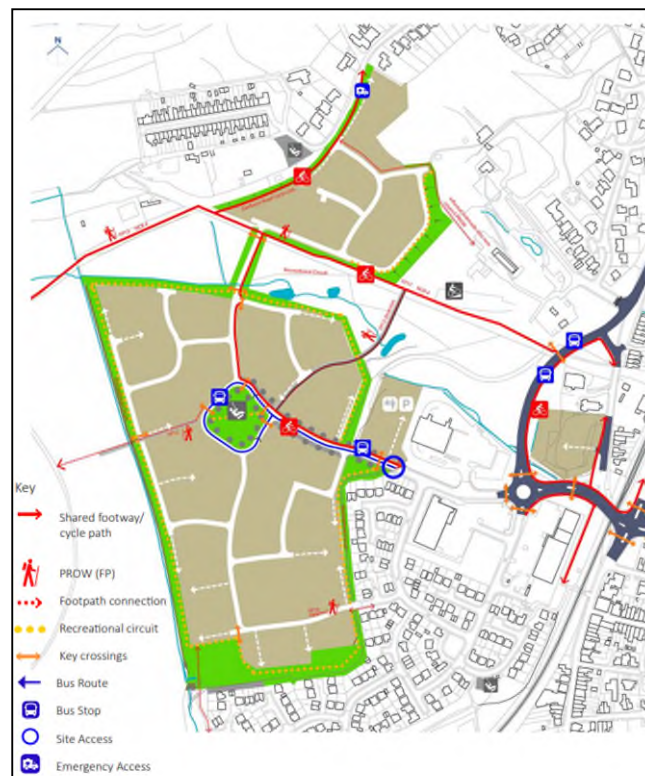


Figure 5 - Movement and Access Plan

The site will be served by one main access point being the continuation of the existing western arm of the Pentre Felin roundabout with the secondary access on Derllwyn Road only for use in times of emergency. A circular bus route has been incorporated into the road and the position of the bus stops have been agreed. Active Travel will be encouraged with a network of walking and cycling routes throughout the development with the necessary crossing facilities both formal and informal.

The parking, access and road layout arrangement have been examined by the Transportation and Engineering Section and generally deemed to be acceptable in the context of the Outline Planning consent and current Welsh Government guidance.

Notwithstanding the above, consideration of the submitted drawings has identified areas of the proposed highway which need addressing although the majority are matters of detail which can be resolved during the agreement of engineering details as part of the Highway adoption process.

There are some more fundamental concerns related to following matters:-

- Vehicle access arrangements for 36 Maesteg Road, Tondu.
- Lack of deflection for users of NCN4 on the downhill approach to the proposed link road Toucan crossing.
- Confirmation of the provision of a verge on the western side of the link road.
- Reverse curvature at the northern tie in of the new link road to Maesteg Road.

however, it is considered that these can be addressed via the imposition of suitable Planning conditions.

Whilst the internal site layout has been detailed with traffic calming measures (which are largely acceptable) it is considered that additional features are required in a number of areas. Again this can be overcome by the inclusion of suitably worded conditions. In order to facilitate the consultation process for the 20mph speed limit a consultation plan will be required to be submitted

In order to ensure that residents, service drivers, delivery drivers and visitors appropriately use the highway at the site access on the link road, the portion of Maesteg Road which will become a cul-de-sac and in the area of the emergency access onto Derllwyn Road and do not park inappropriately on the local highway to the detriment of highway safety, it is considered that some waiting/loading restrictions will be necessary in these areas which can be secured by condition.

Section 106 funding was sought at Outline stage to cover the cost of publication of the Orders (principally in the area around the site access) and whilst this may facilitate these wider areas to be consulted, it may be necessary due to phasing of the development, for the Traffic Orders to be considered separately. In such an instance, there may be additional legal costs to be paid during the highway adoption process.

Members will however note from the table of conditions above that the detail of a number of the highway schemes will need to be agreed before any development commences.

Minimising opportunities for crime to be generated or increased

Local Authorities are under a legal obligation to consider the need to prevent and reduce crime and disorder in all decisions that they take. Crime prevention and fear of crime are social considerations to which regard must be given by Local Planning Authorities. The Secured by Design Officer is also generally amenable to the design, site layout and principles of the development. On the evidence before the Council, the requirement of the policy is addressed.

Avoiding or minimising noise, air, soil and water pollution

Noise and air pollution were not identified as constraints to this development at the Outline application stage despite a number of residents expressing concerns that the additional traffic on the road network would have a negative effect on health and well-being. As Members will know, Local Planning Authorities work closely with pollution control authorities when determining Planning applications and at no point have any adverse

representations been received from the Council's Public Protection Section concerning any issues of noise pollution associated with the development. Noise and dust that may be generated through the construction phases will be controlled through other legislation and conditions requiring the agreement of a Construction Management Plan and hours of operation. As indicated in an earlier part of this report, the site does not fall within an 'Air Quality Management Area'. Member's attention is drawn to conditions 28 and 29 in the table above which will require agreement before any development commences.

In addition, any issues associated with site contamination and possible water pollution will be addressed through the agreement of a site remediation scheme which again will be agreed before any development commences – conditions 24, 25 and 26 in the above table should be referenced.

Incorporating methods to ensure the site is free from contamination (including invasive species)

A protocol detailing the containment, control and removal of Japanese Knotweed and other invasive species on site has been agreed and will be followed throughout all the phases of this development.

Safeguarding and enhancing biodiversity and green infrastructure

Section 40 of the Natural Environment and Rural Communities Act 2006 states *every public authority must in exercising its function have regard, so far as is consistent with the proper exercise of those functions, to the purpose of conserving biodiversity*. This "duty to conserve biodiversity" has been replaced by a "biodiversity and resilience of ecosystems duty" under Section 6 of the Environment (Wales) Act 2016 which came into force on 21 March 2016. Section 6 (1) states *"a public authority must seek to maintain and enhance biodiversity in the exercise of functions in relation to Wales and in so doing promote the resilience of ecosystems, so far as consistent with the proper exercise of those functions*. Section 6(2) goes on to state...*In complying with subsection (1), a public authority must take account of the resilience of ecosystems, in particular:*

- (a) diversity between and within ecosystems;*
- (b) the connections between and within ecosystems;*
- (c) the scale of ecosystems;*
- (d) the condition of ecosystems (including their structure and functioning); and*
- (e) the adaptability of ecosystems.*

Biodiversity and landscape considerations were considered at the Outline stage and a series of planning conditions were imposed

The Outline Planning application was accompanied by a Preliminary Ecological Appraisal (PEA) (Soltys Brewster 2014) and a Level 2 Ecological Survey Report (Soltys Brewster 2016) which established the baseline ecological conditions at the site. Additional surveys for reptiles, bats and Great Crested Newts were subsequently undertaken.

The Green Infrastructure Approach which is promoted by both national and local Planning policies influenced the Master Plan that was approved as part of the Outline consent and which has formed the basis for the Design Principles document that has been agreed prior to the submission of the Reserved Matters. The general approach is to avoid and/or mitigate impact to existing habitats and wildlife following the general principle of retention and protection of as much of the existing green infrastructure as can practically be accommodated within the development. For this development, that includes the development of habitat buffers between the development and the woodland edge. The character of the woodland glades and watercourses on site will be retained and enhanced as access to the wooded areas will be restricted through a sensitive landscape strategy. A connected network of ecological 'buffer zone' corridors is also proposed as part of the

submitted layout, around the perimeter of the development, providing informal open space to be managed for wildlife. These corridors should provide a landscape buffer to sensitive edges and will incorporate wildlife sensitive planting and lighting schemes with particular emphasis on bat foraging routes.

The Preliminary Ecological Appraisal recommended that the small area of more diverse marshy grassland be retained within the buffer or as open space. This forms part of the Southern Park – POS 2 on the submitted layout plans.

Semi natural broadleaved woodland is the dominant habitat in the northern half of the site with several areas of quality and species composition present. Policy ENV6 - Nature Conservation indicates that proposals for development will be required to in the first instance retain, conserve, restore and enhance wherever possible existing woodland. A Tree Survey, Categorisation & Constraints Report accompanied the Outline application and objectively assessed the trees on site and assigns the trees to one of four categories depending on their overall health, size, condition, amenity, cultural and conservation value and their suitability in view of the increased usage that will arise following development. In allowing the Outline consent, the Council acknowledged that the block of woodland covered by the Tree Preservation Order to the south of the cycleway would be retained however, a large block of woodland to the north of the national cycle route where Phase 3 will be developed would be lost. Planning conditions have been imposed requiring the agreement of an Arboricultural Implications Assessment, Tree Protection Plan and Arboricultural Method Statement for the whole site prior to any development commencing. This approach accords with the requirements of Policies ENV5 and ENV6 of the LDP and whilst it is entirely probable that a number of trees will be lost to accommodate the development, this will be controlled through the Planning process. Any trees removed will be offset with new tree planting within the scheme

The presence of a species protected under European or UK legislation is a material consideration when a Local Planning Authority is considering a development proposal which, if carried out, would be likely to result in disturbance or harm to the species or its habitat. The ecology surveys that informed the Outline Planning consent confirmed that the site supports a number of protected/notable species with several trees having bat roosting potential. The Preliminary Ecological Appraisal identified a number of groups of trees with potential for roosting bats both within the main site boundary and along the highway improvement route. Additional survey work will be required and controlled through the conditions on the Outline consent. If a roost is discovered, a licence from Natural Resources Wales (NRW) and appropriate mitigation will be required before any works to the tree/s can be carried out.

Woodland across the site provides potentially suitable habitat for dormice particularly to the east and all the woodlands provide connectivity to larger woodland blocks in the surrounding landscape. The most suitable area of habitat for this species (the eastern woodland) is shown to be retained however, areas are required for removal for the associated highway improvement scheme and an area of lower quality woodland to the north allocated for housing. This area is however considered sub-optimal for dormice but provides habitat connectivity. Due to the lack of records of dormice in the area and suitable habitat being retained, it is not considered that further survey effort is required for dormice providing a sensitive clearance methodology is adhered to. Again, this is a matter that will be agreed before any development commences.

Historical surveys confirmed that the site was found to support a population of Slow Worm and a low population of Grass Snake and toads, all of which receive protection under Section 42 of the NERC Act 2006 and Schedule 7 of the Environment Act 2016. The Local

Authority has a biodiversity duty under both these Acts and must seek to protect these species and enhance the habitats where they occur. The report provided some recommendations that the development should include amphibian friendly features such as incorporating offset curbs/amphibian ladders in drains into the drainage design to prevent these species becoming trapped in gully pots.

In accordance with conditions 18 and 20 of the consent, a Habitat Management Plan (HMP) and Landscape Ecology Management Plan (LEMP) will be submitted before development commences and will set out in detail the mitigation strategies for habitats and species on-site. This will address reptile mitigation although it is the Council's preference for the reptile population to be retained on site. This could potentially be achieved by a combination of measures including new habitat creation within the adjacent Parc Slip Local Nature Reserve.

The applicant's consultant has confirmed that pre-commencement checks will be undertaken if any trees are to be felled that have the potential to support roosting bats. Overall, the proposed approach to the development with recommendations for avoidance and/or mitigation and the general principle of retention, protection and enhancement of as much of the existing green infrastructure as can practically be accommodated within the development is in line with Local Plan Policy and Supplementary Planning Guidance Green Infrastructure Approach. Furthermore, it will ensure that the development contributes to providing an ecosystem for the site and beyond which is both diverse and resilient as required under Section 6 of the Environment (Wales) Act 2016.

Ensuring equality of access by all

Developments must conform to the provisions of the Equality Act 2010 and it is understood that the site and dwellings have been designed to make it accessible for all those who might use the dwellings in the future.

Ensuring that the viability and amenity of neighbouring uses and their users/occupiers will not be adversely affected

National policy confirms that new housing developments should be well integrated with and connected to the existing pattern of settlement and in determining applications for new housing, Local Planning Authorities should ensure that the proposed development does not damage an area's character and amenity.

The agreed Design Principles document sets out a framework for addressing issues of privacy and amenity and establishes criteria which have determined the submitted layout:

- Provide usable amenity space with private gardens being at least the same size as the dwelling footprint.
- Ensure new houses benefit from a satisfactory degree of privacy and daylight.
- Maintain minimum separation distances across the site in particular between habitable rooms. Where two rear habitable rooms face each other such that direct overlooking is physically possible, windows should be 21 metres apart.
- Maintain and enhance the existing landscape bund along the eastern and southern boundaries of the site where the scheme borders existing residential properties. Maintain a minimum width of 8m.
- Locate the recreational route on the internal edge of the bund. Increase planting to help further discourage access to the rear of existing properties and improve the aesthetics of the edge

Changes to the layout have however been necessary to ensure that the objectives set out above are achieved for the housing layout as it relates to the future occupiers of the housing. Nevertheless, the critical issue is whether the impacts of the development on the living conditions and well-being of existing residents are so significant as to be contrary to local policies and guidelines.

The key areas are where the southern and eastern boundaries of Phase 1 – the Main Development Area adjoin existing housing. Whilst the retention of the bund (to be landscaped) along the aforementioned boundaries is an important aspect of the design, a number of 'pinch-points' have been identified which are considered below:

Plot 99/68 Clos Pwll Clai (CPC)

Based on the submitted cross-section drawings, natural ground level rises in favour of the new development and will be increased. The side elevation of 68 CPC which faces the shared boundary with the development site has no side facing habitable room windows and a distance of 13.5m is measured between the side elevation of the respective properties which more than exceeds the guidelines.

The house type originally proposed on this plot included side facing bedroom windows which, in the interests of privacy would be fitted with obscure glazing by virtue of a condition that was to be imposed. The applicant company believed that such glazing would affect the saleability of the property and have changed the housetype to one that has no side facing habitable room windows

Plot 107/61 Clos Pwll Clai (CPC)

As above, the natural ground level rises in favour of the new development and will be increased. Again the side elevation of 61 CPC which faces the shared boundary with the development site has no side facing habitable room windows. In this case, a distance of 14.8m will separate the side elevation with the front elevation of Plot 107 which does have a number of habitable room windows on the ground and first floor. The distance accords with the guidelines but it should be noted that due to the proposed site levels there will be a degree of overlooking. Any loss of privacy is not considered to be sufficient to warrant a change in the submitted layout.

Plot 91/15 and 16 Clos Gwaith Brick (CGB)

Levels are again higher on the development site as confirmed by the Boundary Edge Cross Section drawing. 15 and 16 CGB are detached dwellings that share a rear boundary with the development site. Based on the submitted plans, the front elevation of Plot 91 (a two storey dwelling) will be 26m from the rear facing habitable room windows in 15 and 16 CGB and 16m from their rear boundary fence. Again levels on the development site are higher but even accounting for this change any changes to privacy of the existing residents would be acceptable. Overshadowing and domination of outlook are also not a consideration given the separation distances proposed.

Plots 92/93/94/10 Rhes Leith (RL) and 22 Clos Pwll y Clai (CPC)

The proposed units, again in a more elevated position will be separated from the side boundaries of the rear gardens of 10 RL and 22 CPC by a private drive, recreational path and existing earth bound. A separating distance of between 16-19m is recorded on the submitted layout plan. Whilst there will be a degree of overlooking and the levels of privacy currently enjoyed by the occupiers will be diminished it will not be to such a degree as to conflict with the Council's policies and guidelines.

Plot 88/5 Rhes Leith A distance of 26m will separate the front facing windows of the new dwelling with 5 Rhes Leith. The cross-section below does however confirm the levels of the existing and new dwelling. Privacy will again be compromised but in a manner that will not unacceptably affect the living conditions of residents. New planting along the bund will over time, minimise the impact of the development.



Figure 6 – Cross Section showing relationship between Plot 88 and 9 Rhes Leith

Plot 89/90 to 5 and 6 Rhes Leith (RL)

The new dwellings will overlook the side boundary of 5 and 6 RL, separated by the landscape bund, recreational footpath and a distance of 16.5m to the boundary fence.

Again existing levels of privacy will be compromised but in a manner that will not unacceptably affect the living conditions of residents and in an arrangement that is policy compliant.

Proposed Access Road and 10 Clos Gwaith Brick

This is the relationship between a new section of estate road and the side/rear garden of the aforementioned property - see extract plan below:

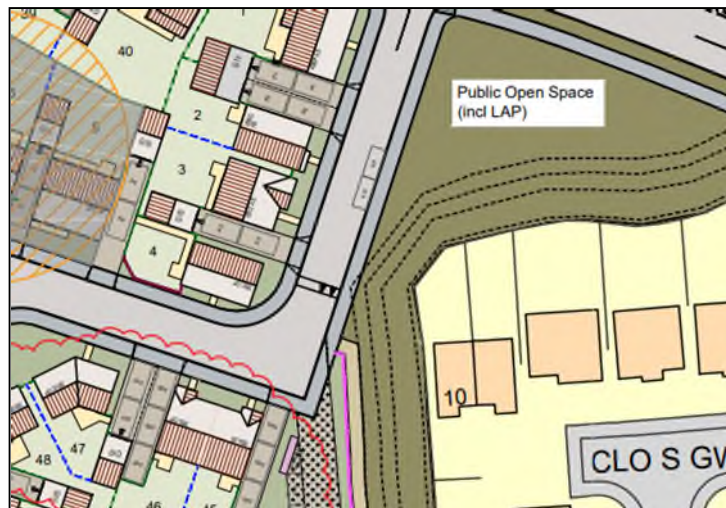


Figure 7 – Cross Section showing relationship between 10 Clos Gwaith Brics and Access Road

Based on the natural topography of the site, the road will cross a higher contour than the garden area of the nearest dwelling which could potentially impact on the living conditions of the occupiers particularly when the development is complete with the associated movement of vehicles and pedestrians. The existing bund is however being retained and will be enhanced with new landscaping. In this location that is critical to ensure that the amenities of the residents are not significantly affected.

The relationship of Plots 334, 337, 338 and 339 to Tawelwch has been considered in an earlier section of the report. The revised layout and updated site sections confirm that the

proposed relationship will comply with the Council's guidelines.

The Old School site which is Phase 2 of this development, lies opposite properties at the southern end of Maesteg Road. A three storey block of flats is proposed at the entrance to this phase directly west of 49 Maesteg Road and the relationship is detailed on the cross-section drawing below. As both the existing and proposed development front the highway, privacy standards will be achieved – a distance of 22m is indicated on the submitted plans.

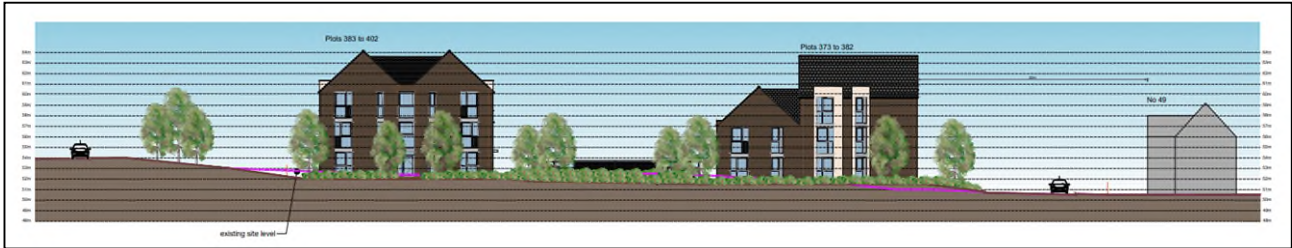


Figure 8 – Cross Section showing relationship between Plots 373-382 and 49 Maesteg Road

This three storey block is a building of considerable mass and scale being over 17m wide and over 11m in height. Whilst the design guidance that informed the Master Plan on the Outline application indicated that 3/4 storey development may be appropriate on this site, it will unquestionably dominate the street scene and the outlook from the front elevation of 49 Maesteg Road. It however stands in relative isolation with landscaped space either side offering some relief in terms of impact on the aforesaid property. In addition, the 22m that separates the front elevations of the properties should ensure that overshadowing is not a factor. Whilst acknowledging that the development will have an impact on 49 Maesteg Road, it is not considered so adverse as to require a further change to the layout of this phase and a scaling down of the block on Plots 373-382. In all other respects, the layout achieves the Council's standards as it relates to safeguarding the amenities of residents on Maesteg Road.

Incorporating appropriate arrangements for the disposal of foul sewage, waste and water

The adequacy of water supply and the sewage infrastructure were significant considerations at the Outline application stage. Members may recall that Dwr Cymru Welsh Water indicated that the development could affect the local drinking water supply system and requested that a Hydraulic Modelling Assessment be agreed before the submission of any Reserved Matters application. That was submitted and agreed in September 2019 and a suitable connection point has been agreed.

The previously submitted Drainage Strategy considers both the foul and surface water strategy including estimated discharge rates, storm water storage requirements and outfall/connection positions. The latest submission confirms that drainage features throughout the site will provide amenity space and biodiversity either by natural forms or by use of specialist landscaping proposals - "SUDS features will be located within the Park Square and Southern Park. The existing ponds located within the woodland area in the north of the site will also function as flood out areas. Drainage for the link road will discharge into the existing watercourse and/or adopted sewers".

The Council's Land Drainage Engineer has confirmed that discussion with the developer are on-going regarding the design and management framework for a comprehensive scheme of drainage for the site. The condition on the Outline consent will require its agreement before any works commence.

It is therefore considered that the proposed development complies with Policy SP2 of the Bridgend County Borough Council Local Development Plan (2013) and advice contained within SPG08, Planning Policy Wales 10 (December 2018) and TAN12: Design

Other Material Considerations – Impact on Heritage Assets

The Council has the responsibility for considering the historic environment as part of their role in determining Planning applications. An archaeological and heritage assessment and addendum accompanied the Outline Planning application. All the heritage assets both designated and non-designated were identified and the impacts of the development assessed.

The Scheduled Monument that is the Remains of Tondu Iron works and the three listed structures lie outside the application site. The historic and modern settings of each of these designated assets was assessed and it was determined that their significance would in no way be adversely affected by the form of development proposed within the site, either in terms of an effect on their physical form/fabric or through change to the contribution made by their setting.

The northern part of the site (Phase 3 on the current submission) is within the Derllwyn Road Conservation Area. The derelict and overgrown land presently makes no contribution to the special character or appearance of the designated area and it was considered that the implementation of the proposed development which could include a layout that is sensitive to the character of the wider Conservation Area, could both enhance this specific portion of the designated area and preserve and enhance the elements of the wider Conservation Area (including its Listed Buildings) which contribute to its character and appearance.

The Authority has a statutory duty to pay special regard to the desirability of preserving Listed Buildings and their settings and a general presumption in favour of the preservation or enhancement of the character or appearance of the Conservation area or its setting.

Changes to the site layout (omission of the garage on Plot 334, the inclusion of chimneys on a number of properties, the use of natural slate to reflect the sensitive position particularly in the context of the adjacent Listed Buildings, glazing bars on the windows, the omission of weatherboarding and the use of re-constituted stone on the boundary walls) are proposed on the latest drawings. Furthermore, the retention of trees and additional landscaping on the north western boundary will soften the impact of the new development on the Conservation Area but also retain the impression of the rural buffer that forms the setting of the Listed Buildings on Park Terrace.

The northern part of the site also lies within the locally identified Area of Archaeological Significance where there is potential for archaeological remains associated with the former 19th and 20th Century ironworks to be found. In the view of the assessor these would potentially be the truncated remains of the former beehive coking ovens, tram roads and coal crusher surviving beneath deposits of coke waste and demolition debris. It is the consultant's opinion that these are very unlikely to be worthy of preservation in situ and no further archaeological investigation is deemed to be warranted at this stage. A Planning condition was however attached to the Outline consent preventing any development on this phase until an archaeological investigation has been carried out.

A draft Heritage Impact Assessment (HIA) has recently been submitted to support this application but also a Listed Building (LB) application will need to be made for the link road and associated infrastructure which are located adjacent to two Grade II Listed structures comprising two parts of the same structure i.e. Bridge Over Incline Plain Tondu (Cadw Ref: 20758 and 19058).



Figure 9 -The Grade II Listed Structures and Bridge

The draft Heritage Impact Assessment (HIA) identifies the following changes to the existing environment in the bridge's surroundings:

- Loss of part of the embankment and former course of the Incline Plane to the east of the bridge;
- Rise in ground level in the footprint of the road, with construction of a raised walkway and small retaining wall to compensate for the change to present levels under the bridge;
- Retention of earth banks immediately south-east of the bridge structure with modern retaining walls to hold back material from the road edge; and
- Construction of fences to restrict access to the listed structure and adjacent banks.

The appearance of the different elements of the proposed development will be as follows:

- The road and adjacent areas will be of modern surfaces with the track running beneath the bridge opening onto a crossing, and with a 2m wide footway running alongside the road to the north
- The raised walkway proposed beneath the bridge will be finished in a composite material. The walkway will be c. 2m wide and will run centrally between the abutments rather than up against them;
- The associated retaining wall will support this structure running between the two abutments of the Listed bridge. The wall will be visible from the walkway and will be of modern concrete formation. It will be constructed in such a way as to have no direct impact on the adjacent abutment walls;
- The earth banks to the immediate south-east of the bridge will be increased slightly in height. As such, the abutments will not be undermined and will be cleared of vegetation. The retaining structure facing the road will be angled in accordance with the batter of the banks, although it will have a modern appearance distinct from the stone finish of the listed structure; and
- The fence will be of a modern appearance constructed in steel.

In addition to the above, the bridge will be subject to remedial and conservation works.

The applicant's heritage assessor has concluded that the adverse effects from the loss of aspects of the bridge's setting will be outweighed by the benefits to its significance from the proposed conservation works which will safeguard its future integrity as a historic asset.

The relevant factors to consider and weigh in the assessment in this case include;

- The significance of the asset and the contribution the setting makes to that significance
- The prominence of the historic asset
- Non visual factors affecting the setting of the historic asset e.g. noise
- Inter-visibility with other historic or natural features

The bridge is listed as a building of special architectural or historic interest as a mostly intact structure of a mid C19 industrial transport complex and representative of the second stage of development under John Brogden of the Tondy Ironworks founded in 1830s by Sir Robert Price as identified by the applicant has evidential and historical significance and value. The structure is relatively visually secluded and, with the exception of the incline plane, has limited inter-visibility with related historic assets in the local landscape. The site has a relatively tranquil setting.

The impact of the proposed development will result in:

- The loss of part of the embankment and former course of the incline to the east of the bridge
- A rise in ground level in the footprint of the proposed road realignment and the construction of a raised walkway and small retaining wall to compensate for change to present levels under the Bridge structure
- The retention of earth banks SE of the bridge structure and modern retaining walls
- The erection of fencing

The draft heritage impact assessment states that the incline plane/path, former track bed, earthwork banks and bridge to the South East make a moderate contribution to the significance and setting of the listed building.

It is considered that, on balance, any potential harm caused by the proposed works outlined above may be limited to an acceptable level by the careful selection of proposed materials that are not harmful to the setting of the historic asset and may be outweighed by the proposal subject to the timely sensitive repair and restoration of the bridge structure along with a proposal for the interpretation of the significance of the historic asset and its relationship with other features in the surrounding landscape.

It is recommended that an appropriate condition is therefore included to ensure the timely protection and subsequent repair of the structure in the interests of the preservation of the buildings of special architectural or historic interest. The proposed development whilst increasing the prominence and visibility of the historic asset, will also allow a fuller appreciation of the significance of the asset and its context.

Given the overall benefit of the development proposals to the significance of the two Listing designations, the proposals accord with legislation.

CONCLUSION

Section 38(6) of the 2004 Act requires that regard is to be had to the Development Plan for the purposes of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise. Factors to be taken into account in making Planning decisions (material considerations) must be Planning matters that is they must be relevant to the

regulation of the development and use of land in the public interest towards the goal of sustainability.

In this case the site is specifically allocated for residential in the Local Development Plan (LDP) adopted by this Council in 2013. The LDP is a robust document and was subject to considerable scrutiny, consultation and public examination by an outside planning inspector. The development proposed as part of this application is in accord with this allocation.

It is also considered in view of the comprehensive appraisal outlined in this report that this Reserved Matters submission accords with the Outline Planning consent. The placemaking objectives of national policy and the high quality development that is required by local policy will be achieved by virtue of the design being of an appropriate scale and size, good connections to walking, cycling and public transport connections, according with the framework of measures that seek to minimise noise, air and water pollution and the agreed areas of ecological enhancement.

The proposed development will make a significant contribution to housing land supply, is located in a sustainable location in terms of access to services, amenities and public transport and would not cause unacceptable harm to its surroundings. There is no evidence before the Council which would suggest there would be any unacceptable impacts on local economic, social and environmental infrastructure. The proposed development would not undermine the principles of sustainable development or the creation of cohesive communities, which forms the basis of local and national Planning policy.

Section 3 of the Wellbeing of Future Generations (Wales) Act 2015 imposes a duty on public bodies to carry out sustainable development in accordance with sustainable development principles to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). The well-being goals identified in the Act are:

- A prosperous Wales
- A resilient Wales
- A healthier Wales
- A more equal Wales
- A Wales of cohesive communities
- A Wales of vibrant culture and thriving Welsh language
- A globally responsible Wales

The duty has been considered in the assessment of this application. It is considered that there would be no significant or unacceptable impacts upon the achievement of well-being goals/objectives as a result of the proposed development

The application is therefore recommended for approval

RECOMMENDATION

That permission be GRANTED subject to the following conditions:

1.	The development shall be carried out in accordance with the following approved plans and documents: plan numbers: Location Plan 1828-LP-01
----	---

External Works Drawing 1 of 5 – 1828-EW-01 – Revision C
External Works Drawing 2 of 5 – 1828-EW-02 – Revision C
External Works Drawing 3 of 5 – 1828-EW-03 – Revision C
External Works Drawing 4 of 5 – 1828-EW-04 – Revision C
External Works Drawing 5 of 5 – 1828-EW-05 – Revision B

House Finishes Layout Drawing 1 of 5 – 1828-HF-01 – Revision D
House Finishes Layout Drawing 2 of 5 – 1828-HF-02 – Revision D
House Finishes Layout Drawing 3 of 5 – 1828-HF-03 – Revision D
House Finishes Layout Drawing 4 of 5 – 1828-HF-04 – Revision C
House Finishes Layout Drawing 5 of 5 – 1828-HF-05 – Revision B

Parking Strategy 1828-PS-01 – Revision D

Site Cross Sections 1828-SCS-01 – Revision A
Street Cross Sections 1828-SCS-01 – Revision A
Site Layout 1828-TP-01 – Revision C
Site Layout 1828-TP-02 – Revision C
Site Layout 1828-TP-03 – Revision C
Site Layout 1828-TP-04 – Revision C
Site Layout 1828-TP-05 – Revision B

Overall Site Layout 1828-TP-1000 – Revision F

Phases 1 & 2 of the development shall be constructed in the following schedule of materials:

- Ibstock Welbeck Autumn Antique Facing Brick
- Ibstock Minster Beckstone Mix Facing Brick
- Forticrete Shearstone Pentstone Cottage walling stone
- Weber Pral M through Colour Render – Chalk
- Weber Pral M through Colour Render - Cream
- Weber Pral M through Colour Render – Granite Grey
- Cedral Weatherboard – C01 White
- Forticrete Cast Stone – Sherwood Forticrete Cast Stone – Bath
- Marley Duo Edgemere interlocking slate tile – Smooth Grey

House Types: 2B3-ST1-1; 2B3-ST2-1 3B12-ST1-1; 3B12-ST2-1; 3B12-ST2-1; 3B12-ST3-1; 3B12-ST5-1; 3B13-ST1-1; 3B13-ST2-1; 3B13-ST5-1; 3B16-ST1-1; 3B16-ST2-1; 3B18-ST1-1; 3B18-ST2-1; 3B18-ST4-1; 3B19D-ST1-1; 3B19D-ST2-1; 3B19D-ST5-1; 3B19S-ST1-1; 3B19S-ST2-1; 3B19S-ST4-1; 3B19S-ST5-1; 3B20SP-ST1-1; 3B20-ST1-1; 3B20-ST2-1; 3B4-ST1-1; 3B4-ST2-1A; 3B4-ST5-1; 3B5E-ST1-1; 3B5E-ST2-1; 3B5G-ST1-1; 3B5G-ST2-1; 3BC-ST1-1; 3BC-ST2-1; 3BC-ST4-1A; 4B13SP-ST3-1; 4B13SP-ST4-1; 4B13-ST1-1; 4B13-ST2-1; 4B13-ST3-1; 4B13-ST5-1; 4B17-ST1-1; 4B17-ST2-1; 4B17-ST3-1; 4B18N-ST1-1; 4B18N-ST2-1A; 4B18N-ST4-1A; 4B19SP-ST3-1; 4B19-ST1-1; 4B19-ST2-1; 4B19-ST3-1; 4B19-ST5-1; 4B1-ST1-1; 4B1-ST2-1; 4B1-ST3-1; 4B1-ST4-1; 4B1-ST5-1; 4B2SP-ST1-1; 4B2SP-ST2-1; 4B2SP-ST3-1; 4B2SP-ST5-1; 4B2-ST1-1; 4B2-ST2-1A; 4B2-ST3-1; 4B2-ST5-1; 4B3-ST1-1; 4B3-ST2-1A; 4B3-ST4-1A; 4B6-ST1-1; 4B6-ST2-1; 4B6-ST4-1A; 4B7-ST1-1; 4B7-ST2-1; 4B7-ST4-1A; 4BC-ST1-1; 4BC-ST2-1; 4BC-ST4-1A; 641-ST1-1A; 641-ST2-1;

Close Boarded FenceEN01

	<p>Post and Wire Fence – EN02 Timber and Knee Rail – EN03 Screen Wall – EN04 Close Board Gate – EN05 Hoop Top Railings – EN06 Stone Wall – EN010 Enclosures – EN01 Garages – G10-B-01</p> <p>Detailed Soft Landscaping Proposals 1 to 20 received on 3rd September 2020</p> <p>Footpath Diversion Plan – FDP-01 – Revision C 17-03-20</p> <p>7066-WSP-DR-C-20111 General arrangement (Sheet 1 of 3) 1:500 A1 P07 7066-WSP-DR-C-20112 General arrangement (Sheet 2 of 3) 1:500 A1 P05 7066-WSP-DR-C-20113 General arrangement (Sheet 3 of 3) 1:500 A1 P04</p> <p>7066-WSP-DR-C-20131 Proposed contours (Sheet 1 of 2) 1:500 A1 P02 7066-WSP-DR-C-20132 Proposed contours (Sheet 2 of 2) 1:500 A1 P01</p> <p>7066-WSP-DR-C-10111 General arrangement (Sheet 1 of 4) 1:500 A1 P06 7066-WSP-DR-C-10112 General arrangement (Sheet 2 of 4) 1:500 A1 P07 7066-WSP-DR-C-10113 General arrangement (Sheet 3 of 4) 1:500 A1 P07 7066-WSP-DR-C-10114 General arrangement (Sheet 4 of 4) 1:500 A1 P07</p> <p>7066-WSP-DR-C-10115 General arrangement details (Sheet 1 of 2) 1:200 A1 P02 7066-WSP-DR-C-10116 General arrangement details (Sheet 2 of 2) 1:200 A1 P03</p> <p>7066-WSP-DR-C-10120 Typical carriageway sections NTS A1 P02 7066-WSP-DR-C-10125 Vehicle tracking 1:250 A1 P04</p> <p>Reason: To avoid doubt and confusion as to the nature and extent of the approved development.</p>
<p>2.</p>	<p>Prior to the commencement of the development of Phase 3 as identified in the Design Principles document, detailed specification for, or samples of, the materials to be used in the construction of the external surfaces of the buildings hereby permitted shall be submitted to and agreed in writing by the Local Planning Authority. The detailed specifications shall be submitted no less than 28 days prior to works on the phase commencing. Development shall be carried out in accordance with the agreed details</p> <p>Reason: To ensure that the proposed materials of construction are appropriate for use on the development to protect the visual amenities of the area</p>
<p>3.</p>	<p>Prior to the commencement of each development phase identified in the Design Principles document, a detailed scheme including a timetable for implementation, all items of play, associated grounds works and hard and soft landscaping works for the proposed Local Area of Play and Local Equipped Play Area on the respective phases shall be submitted to and agreed in writing by the Local Planning Authority. The detailed specifications shall be submitted no less than 28 days prior to works on the phase commencing. Development shall be carried out in accordance with the agreed details.</p> <p>Reason: In the interests of the residential amenities of future occupants.</p>

4.	<p>Prior to the commencement of each development phase identified in the Design Principles document, details of the proposed floor levels of the buildings in relation to existing ground levels and the finished levels of the site have been submitted to and agreed in writing by the Local Planning Authority. The details shall be submitted no less than 28 days prior to works on the phase commencing. Development shall be carried out in accordance with the agreed details.</p> <p>Reason: To ensure that development relates appropriately to the topography of the site and the surrounding area.</p>
5.	<p>No development shall commence on any phase of the development until the applicant or their agents or successors in title has secured agreement for a written scheme of historic environment mitigation which has been submitted by the applicant and approved by the Local Planning Authority. The approved site investigation works shall be implemented prior to the commencement of development on Phase 3 or any infrastructure scheme or strategic landscaping area. Thereafter, the programme of work will be fully carried out in accordance with the requirements and standards of the written scheme and the developer shall afford access at all reasonable times during construction to a nominated archaeologist for the purpose of observing the excavations and recording items of interest and finds.</p> <p>Reason: To safeguard the heritage assets that may be buried beneath ground in accordance with national and local planning policy.</p>
6	<p>No structure, erection or planting exceeding 0.6 metres in height above adjacent carriageway level shall be placed within the required vision splay areas at any time.</p> <p>Reason: In the interests of highway safety</p>
7.	<p>No development shall commence on site until a Construction Management Plan has been submitted to and agreed in writing by the Local Planning Authority. The Construction Management Plan shall be divided into phases of construction and include:-</p> <ul style="list-style-type: none"> • Timing and routing of construction and delivery vehicles • Provision of a temporary compound for the storage of construction materials and plant; • Parking for contractor's vehicles throughout the construction; • Parking of vehicles for site operatives and visitors; • Wheel washing to prevent mud and debris from the construction traffic being carried out onto the existing highway network within the existing Estate; • The methodology and details required as to how the Listed bridge will be protected both during the construction phases (including the removal of vegetation) and from noise/vibration caused by construction and future vehicular traffic <p>The construction works shall thereafter be undertaken in accordance with the agreed Construction Management Plan throughout the construction phases.</p> <p>Reason: In the interests of highway safety.</p>
8	<p>Notwithstanding the approved layout plan no development shall commence in respect of the link road until a scheme for vehicle access arrangements for 36 Maesteg Road has</p>

	<p>been submitted to and approved in writing by the Local Planning Authority. The vehicle access arrangements shall be implemented as agreed in permanent materials before the link road is brought into beneficial use.</p> <p>Reason: In the interests of highway safety.</p>
9	<p>Notwithstanding the approved layout plan no development shall commence in respect of the link road until a revised scheme for NCN Route 4 pedestrian and cycle crossing arrangements over the link road in the vicinity of the Listed abutment structure has been submitted to and approved in writing by the Local Planning Authority. The crossing arrangements shall be implemented as agreed in permanent materials before the link road is brought into beneficial use.</p> <p>Reason: In the interests of highway safety.</p>
10	<p>Notwithstanding the approved layout plan, no development shall commence in respect of the link road until a scheme for a continuous margin on the western side of the link road between the site access roundabout and the agreed NCN Route 4 pedestrian and cycle crossing (condition 9 refers) and from the agreed crossing to the junction for Ffordd Haearn (Iron Way) has been submitted to and approved in writing by the Local Planning Authority. Where achievable the margin will be 2.5m wide and shall be laid out before the link road is brought into beneficial use.</p> <p>Reason: In the interests of highway safety</p>
11	<p>Notwithstanding the approved layout plans no development shall commence in respect of the link road until a revised arrangement at the northern tie-in point of the link road with Maesteg Road (south east of 5 Ffordd Haearn (Iron Way)) has been submitted to and approved in writing by the Local Planning Authority. The link road tie-in arrangements shall be implemented as agreed in permanent materials before the link road is brought into beneficial use.</p> <p>Reason: In the interests of highway safety.</p>
12	<p>Prior to the commencement of each development phase identified in the Design Principles document, a comprehensive scheme for traffic calming restricting 85% tile traffic speeds to 20mph shall be submitted to the Local Planning Authority. The scheme as agreed in writing by the Local Planning Authority shall be implemented before any dwelling in the respective phases is occupied.</p> <p>Reason: In the interests of highway safety.</p>
13	<p>The proposed means of access to the temporary car park shall be laid out in permanent materials for a distance of no less than 5m from the back of the active travel route and with cycle vision splays of 2.4m (measured from the back of the active travel route) x 15m to the West 10m to the East (or as shown on drawing 7066-WSP-DR-C-10114 Rev P07) before the car park is brought into beneficial use and retained as such thereafter.</p> <p>Reason: In the interests of highway safety.</p>

14	<p>The proposed temporary car park shall be laid out before the commencement of Phase 2 of the development.</p> <p>Reason: In the interests of highway safety.</p>
15	<p>The proposed means of access to Plots 248 – 257 and 285 – 288 (inclusive) shall be laid out with the cycle vision splays shown on drawing 7066-WSP-DR-C-10112 Rev P07 cycle vision splays of before the dwellings are brought into beneficial use and retained as such thereafter in perpetuity.</p> <p>Reason: In the interests of highway safety.</p>
16	<p>The proposed junction of the pedestrian path adjoining Plots 284 and 285 with the active travel route fronting Plots 255-289 shall be laid out with pedestrian vision splays of 1m (measured from the back of active travel route) 15m to the West 10m to the East as shown on drawing 7066-WSP-DR-C-10112 Rev P07 before the pedestrian route is brought into beneficial use and retained as such thereafter in perpetuity.</p> <p>Reason: In the interests of highway safety.</p>
17	<p>Within 3 months of the date of this decision, a scheme of waiting and loading restrictions along the site access road fronting the “waste transfer station” from the site access roundabout to the junctions adjacent to Plots 1 and 289 shall be submitted to the Local Planning Authority. The scheme as agreed in writing by the Local Planning Authority shall include for road markings and signage and shall be implemented within 6 months of the occupation of the dwellings on Plots 1 and 289.</p> <p>Reason: In the interests of highway safety.</p>
18	<p>No development of the link road shall commence until a scheme of waiting and loading restrictions along the link road from the site access roundabout to the northern tie-in with Maesteg Road (south east of 5 Ffordd Haearn (Iron Way)) has been submitted to and agreed in writing by the Local Planning Authority. The approved scheme shall include for road markings and signage. Such scheme shall be implemented as approved by the Local Planning Authority within 6 months of the date of the opening of the link/by-pass road.</p> <p>Reason: In the interests of highway safety.</p>
19	<p>No development of the link road shall commence until a scheme of waiting and loading restrictions along Maesteg Road between numbers 16 and 49 including the junction with the link road has been submitted to and agreed in writing by the Local Planning Authority. The approved scheme shall include for road markings and signage. Such scheme shall be implemented as approved by the Local Planning Authority within 6 months of the date of the opening of the link/by-pass road.</p> <p>Reason: In the interests of highway safety.</p>

20	<p>No development of Phase 3 shall commence until a scheme of waiting and loading restrictions along Road 7 and Derllwyn Road in the vicinity of the emergency access has been submitted to and agreed in writing by the Local Planning Authority. The approved scheme shall include for road markings and signage. Such scheme shall be implemented as approved by the Local Planning Authority prior to any dwelling on Phase 3 of the development being occupied or within 6 months of the emergency access being constructed, whichever is the sooner.</p> <p>Reason: In the interests of highway safety.</p>
21	<p>No dwelling shall be occupied until the shared and individual driveways and parking bays serving the dwelling have been laid out as approved and completed in permanent materials at gradients that do not exceed 8.33% (1 in 12) in accordance with the approved layout. The parking bays shall be retained thereafter for parking purposes in perpetuity.</p> <p>Reason: To ensure the provision and retention of sufficient off street parking in the interests of highway safety.</p>
22	<p>No dwelling shall be occupied until the individual or shared driveways serving the dwelling has been laid out with a 1m x 1m pedestrian vision splays. The vision splays shall be retained as such thereafter in perpetuity.</p> <p>Reason: In the interests of highway safety.</p>
23	<p>The proposed junction and forward vision splays as shown on drawing numbers: 7066-WSP-DR-C-10111 Rev P06; 7066-WSP-DR-C-10112 Rev P07; 7066-WSP-DR-C-10113 Rev P07; 7066-WSP-DR-C-10114 Rev P07 and 7066-WSP-DR-C-20111 Rev P07 shall be provided in both directions prior to the beneficial use of any unit served by that junction and shall be retained as such thereafter in perpetuity.</p> <p>Reason: In the interests of highway safety</p>
24	<p>The proposed temporary car park access junction shall be laid out in accordance with Drawing 7066-WSP-DR-C-10114 Rev P07 prior to the car park being brought into beneficial use.</p> <p>Reason: In the interests of highway safety.</p>
25.	<p>* THE FOLLOWING ARE ADVISORY NOTES NOT CONDITIONS *</p> <p>Section 38(6) of the 2004 Act requires that if regard is to be had to the Development Plan for the purposes of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise. Factors to be taken into account in making Planning decisions (material considerations) must be Planning matters that is they must be relevant to the regulation of the development and use of land in the public interest towards the goal of sustainability.</p>

In this case it is considered that this Reserved Matters submission accords with the Outline Planning consent. The placemaking objectives of national policy and the high quality development that is required by local policy will be achieved by virtue of the design being of an appropriate scale and size, good connections to walking, cycling and public transport connections, according with the framework of measures that seek to minimise noise, air and water pollution and the agreed areas of ecological enhancement.

The proposed development will make a significant contribution to housing land supply, is located in a sustainable location in terms of access to services, amenities and public transport and would not cause unacceptable harm to its surroundings. There is no evidence before the Council which would suggest there would be any unacceptable impacts on local economic, social and environmental infrastructure. The proposed development would not undermine the principles of sustainable development or the creation of cohesive communities, which forms the basis of local and national Planning policy.

Section 3 of the Wellbeing of Future Generations (Wales) Act 2015 imposes a duty on public bodies to carry out sustainable development in accordance with sustainable development principles to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs (Section 5). The well-being goals identified in the Act are:

- A prosperous Wales
- A resilient Wales
- A healthier Wales
- A more equal Wales
- A Wales of cohesive communities
- A Wales of vibrant culture and thriving Welsh language
- A globally responsible Wales

The duty has been considered in the assessment of this application. It is considered that there would be no significant or unacceptable impacts upon the achievement of well-being goals/objectives as a result of the proposed development

The proposed works for the repair and alteration of the Listed structures will be considered in detail as part of a separate application for Listed Building Consent. Prior to the submission of the Listed Building Consent application, the applicant must seek pre-application advice from the Local Planning Authority and the Building Conservation and Design Team and the application must include an updated Heritage Impact Assessment as follows:

- The need for a summary options appraisal that have been considered since the granting of Outline consent for the site to demonstrate why this is the option that in the applicant's judgement "minimises any harm" to the structure, with reference to Cadw's Heritage Impact Assessment Guidance)
- To supplement the photographs submitted, a plan indicating the inter-relationship of the historic elements should be included in the Heritage Impact Assessment to assist with the assessment of harm to the Listed structure and its significance
- Sensitive repair and restoration of the structure is welcomed but a detailed schedule of repairs and alterations and a maintenance/management plan will be required as part of any Listed Building Consent application.
- Further consideration needs to be given to the principle and selection of the use of "modern materials" within the context of the Listed structure as the embankment and revetment walls form part of the Listing including methodology and details of the fixing/anchoring of fencing and the method of how the embankment walls will be retained.

- Due to the poor condition of the structure, an updated structural survey of the structure will be required along with a programme of works and methodology to ensure the protection of the structure during construction works and early phasing of repair works.
- Assessment of and proposed mitigation (including a detailed methodology) of the impact of the removal of trees and any vegetation affecting the Listed structure.
- The proposed location and details of interpretation proposals at their site
- An assessment of potential vibration damage to the structure both during construction and due to proximity of vehicular traffic if approved – what measures will be taken to protect the structure during construction and thereafter.

There is a requirement for the timely and sensitive repair and restoration of the bridge structure along with a proposal for the interpretation of the significance of the historic asset and its relationship with other features in the surrounding landscape.

The Highway Authority will require the Developer to enter into legally binding Section 111 Licence Agreement including an appropriate bond to secure the proper implementation of the proposed highway works and the adoption of the same as part of the maintainable highway. The commencement of the works on or abutting the existing maintainable highway will not be permitted until such time as the Agreement has been concluded.

Street nameplates reflecting the official street name allocated by the Council shall be erected by the developer at locations and to a specification to be agreed with the Council prior to beneficial occupation of the first dwelling house in the street that has been so allocated.

An information pack containing public transport information including timetables shall be provided by the developer on occupation of each residential unit.

JONATHAN PARSONS
GROUP MANAGER PLANNING & DEVELOPMENT SERVICES

Background Papers

None.